



Cluck Creek Trail

Cluck Creek is one of two major creek corridors in this sector of the City. Placing a trail along this drainage corridor will connect several neighborhoods to each other, as well as to Creekside Park, Cluck Creek Park and several major employers in the City. This corridor makes for an excellent off street trail opportunity. The corridor is wide enough to easily accommodate a trail.

CITY OF CEDAR PARK - EVALUATION CRITERIA for HIKE & BIKE CORRIDORS			
Corridor Name: Cluck Creek Trail		Score: B	
Type: Trail		Length: 5,670 ft.	
Evaluation Criterion	Importance	Total Pts Available	Points
Connectivity	20%	20	12
Schools		6	6
Trail-to-Trail		6	6
Parks & Other Amenities		4	4
Major Retail		2	2
Major Employers		2	2
Availability	15%	15	15
City Owned		15	15
Entity Owned		10	10
Privately Owned		0	0
- Single Owner		6	6
- Common Ownership (HOA)		4	4
- Multiple Owners		2	2
Proximity to SFR*	20%	20	13
Width of Corridor - Separation		15	15
Elevation - Visibility from Above		-5	-5
Buffers		10	3
- Visual		6	1.75
- Vegetation		1.75	1.75
- Fencing		1.75	1.75
- Berms		2.5	2.5
- Noise		4	1.25
- Vegetation		1.25	1.25
- Fencing		1.25	1.25
- Berms		1.5	1.5
Current Conditions	10%	10	7
Ex. Trail or Sidewalk & Used		10	10
No Trail or Sidewalk, but Used		7	7
No Trail or Sidewalk & Un-Used		5	5
Site Details	5%	5	5
Usable w/out Improvement		5	5
Un-Usable w/ out Improvement		2	2
Public Opinion of APO**	25%	25	15
Support (75%+)		25	25
Oppose (<25%)		0	0
Mix - For vs. Against		10 - 15	15
Jurisdiction	5%	5	5
In City		5	5
In ETJ		3	3
Total	100%	100	72

* Single Family Residential Property ** Adjacent Property Owners





CITY OF CEDAR PARK - EVALUATION CRITERIA for HIKE & BIKE CORRIDORS
 Corridor Name: Buttercup Creek Blvd. Score: A
 Type: Sidewalk Length: 2,345 ft.

Evaluation Criterion	Importance	Total Pts Available	Points
Connectivity	20%	20	12
Schools		6	6
Trail-to-Trail		6	6
Parks & Other Amenities		4	4
Major Retail		2	2
Major Employers		2	2
Availability	15%	15	15
City Owned		15	15
Entity Owned		10	10
Privately Owned			0
- Single Owner		6	6
- Common Ownership (HOA)		4	4
- Multiple Owners		2	2
Proximity to SFR*	20%	20	18
Width of Corridor - Separation		15	15
Elevation - Visibility from Above		-5	3
Buffers		10	10
- Visual		6	1.75
- Vegetation		1.75	1.75
- Fencing		1.75	1.75
- Berms		2.5	2.5
- Noise		4	1.25
- Vegetation		1.25	1.25
- Fencing		1.25	1.25
- Berms		1.5	1.5
Current Conditions	10%	10	7
Ex. Trail or Sidewalk & Used		10	10
No Trail or Sidewalk, but Used		7	7
No Trail or Sidewalk & Un-Used		5	5
Site Details	5%	5	2
Usable w/out Improvement		5	5
Un-Usable w/ out Improvement		2	2
Public Opinion of APO**	25%	25	25
Support (75%+)		25	25
Oppose (<25%)		0	0
Mix - For vs. Against		10 - 15	10 - 15
Jurisdiction	5%	5	5
In City		5	5
In ETJ		3	3
Total	100%	100	84

* Single Family Residential Property ** Adjacent Property Owners

Buttercup Creek Blvd.

There are two segments along Buttercup Creek Blvd. where the sidewalk stops abruptly. The sidewalk on the south side of the street, towards Bell St. needs to be expanded. Also the sidewalk on the north side of the street ends once it reaches the cave preserves. Because of the restrictions of the cave preserves, it might not be possible to place a concrete sidewalk path along this area of Buttercup Creek Blvd. The City should work with the Texas Cave Conservancy to make sure that the caves are preserved, and that any future sidewalk expansion does not disrupt any protected species.

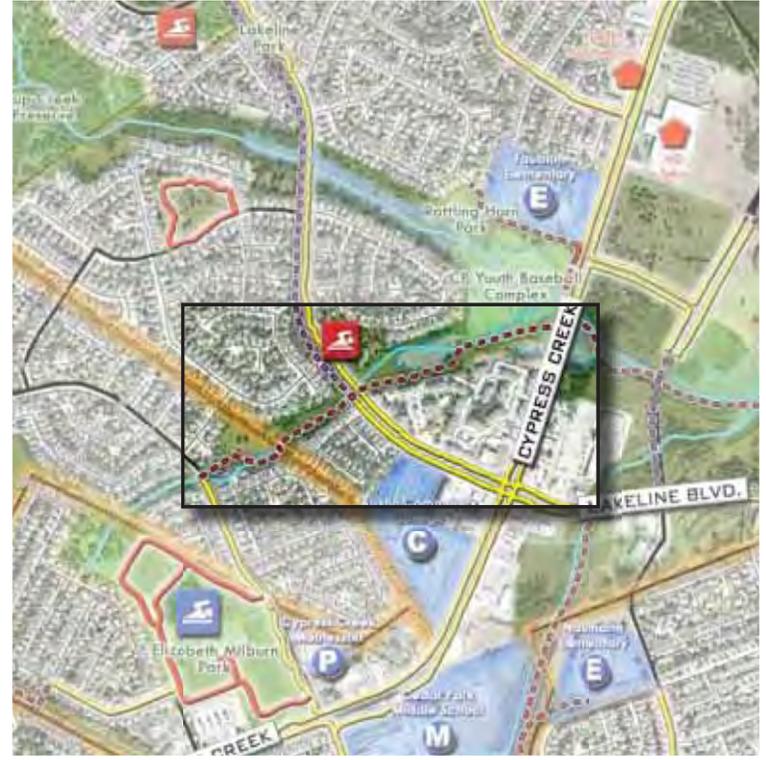


South Buttercup Creek Trail (Sun Chase Blvd. to Faubion Elementary School)

This drainage corridor connects Faubion Elementary School, the Cedar Park Youth Baseball Complex, an HOA swimming pool, and a multi-family apartment complex. A trail along this drainage corridor will provide a natural setting for people to experience the outdoors. The corridor is wide enough to accommodate a trail while not disturbing the surrounding neighborhoods. This was one of the most highly supported trail opportunities discussed during the public input process.

CITY of CEDAR PARK - SELECTION CRITERIA for HIKE & BIKE CORRIDORS			
Corridor Name: South Buttercup Creek Trail		Grade: A	
Type: Trail		Length: 5,693 ft.	
Selection Criterion	Importance	Total Pts Available	Points
Connectivity	20%	20	10
Schools		6	6
Trail-to-Trail		6	
Parks & Other Amenities		4	4
Major Retail		2	
Major Employers		2	
Availability	15%	15	15
City Owned		15	15
Entity Owned		10	
Privately Owned			0
- Single Owner		6	
- Common Ownership (HOA)		4	
- Multiple Owners		2	
Proximity to SFR*	20%	20	21
Width of Corridor - Separation		15	15
Elevation - Visibility from Above		-5	
Buffers		10	6
- Visual		6	3.5
- Vegetation		1.75	1.75
- Fencing		1.75	1.75
- Berms		2.5	
- Noise		4	2.5
- Vegetation		1.25	1.25
- Fencing		1.25	1.25
- Berms		1.5	
Current Conditions	10%	10	7
Ex. Trail or Sidewalk & Used		10	
No Trail or Sidewalk, but Used		7	7
No Trail or Sidewalk & Un-Used		5	
Site Details	5%	5	2
Usable w/out Improvement		5	
Un-Usable w/ out Improvement		2	2
Public Opinion of APO**	25%	25	25
Support (75%+)		25	25
Oppose (<25%)		0	
Mix - For vs. Against		10 - 15	
Jurisdiction	5%	5	5
In City		5	5
In ETJ		3	
Total	100%	100	85

* Single Family Residential Property ** Adjacent Property Owners





CITY OF CEDAR PARK - EVALUATION CRITERIA for HIKE & BIKE CORRIDORS
 Corridor Name: Lakeline Village PUD Park
 Type: Trail
 Length: 15,455 ft.
 Score: A

Evaluation Criterion	Importance	Total Pts Available	Points
Connectivity	20%	20	16
Schools		6	6
Trail-to-Trail		6	6
Parks & Other Amenities		4	4
Major Retail		2	
Major Employers		2	
Availability	15%	15	15
City Owned		15	15
Entity Owned		10	
Privately Owned			0
- Single Owner		6	
- Common Ownership (HOA)		4	
- Multiple Owners		2	
Proximity to SFR*	20%	20	26
Width of Corridor - Separation		15	15
Elevation - Visibility from Above		-5	
Buffers		10	11
- Visual		6	6
- Vegetation		1.75	1.75
- Fencing		1.75	1.75
- Berms		2.5	2.5
- Noise		4	5
- Vegetation		1.25	1.25
- Fencing		1.25	1.25
- Berms		1.5	2.5
Current Conditions	10%	10	5
Ex. Trail or Sidewalk & Used		10	
No Trail or Sidewalk, but Used		7	
No Trail or Sidewalk & Un-Used		5	5
Site Details	5%	5	2
Usable w/out Improvement		5	
Un-Usable w/ out Improvement		2	2
Public Opinion of APO**	25%	25	25
Support (75%+)		25	25
Oppose (<25%)		0	
Mix - For vs. Against		10 - 15	
Jurisdiction	5%	5	5
In City		5	5
In E.T.J.		3	
Total	100%	100	94

* Single Family Residential Property ** Adjacent Property Owners

Lakeline Village PUD Park

One of the most highly desired trail corridors that came out of the public input process was the development of the Lakeline Village PUD Park and trails that encircle the lake. Buttercup Creek connects to Lake Cedar Park, and proposed trails are recommended along the creek from the lake to Cypress Creek Road.

Once this park is developed, it will likely be a major attraction in the City. Trails from the surrounding neighborhoods should connect to it. Also, there is potential to connect the trails around the lake to the existing Brushy Creek Regional Trail system by crossing over Bell St. at either Little Elm Trail or Avery Ranch Blvd.



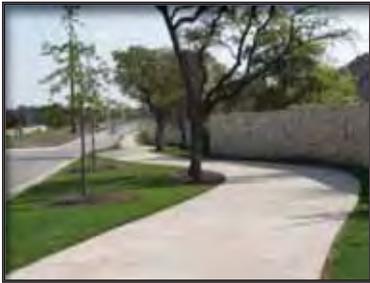
Lakeline Village Powerline Corridor

There is a powerline corridor that connects from Cypress Creek Rd. to Lakeline Blvd. The powerline passes through the future Lakeline Village PUD Park. Constructing a trail along this powerline corridor will connect the two major arterial roads as well as the future park. Powerline corridors are generally wide enough to construct a trail for multiple users, and are already mowed so the maintenance of the trail has relatively little impact.

CITY OF CEDAR PARK - EVALUATION CRITERIA for HIKE & BIKE CORRIDORS			
Corridor Name: Lakeline Village Powerline Corridor		Score: B	
Type: Trail		Length: 6,775 ft.	
Evaluation Criterion	Importance	Total Pts Available	Points
Connectivity	20%	20	6
Schools		6	
Trail-to-Trail		6	
Parks & Other Amenities		4	4
Major Retail		2	
Major Employers		2	2
Availability	15%	15	10
City Owned		15	
Entity Owned		10	10
Privately Owned			0
- Single Owner		6	
- Common Ownership (HOA)		4	
- Multiple Owners		2	
Proximity to SFR*	20%	20	21
Width of Corridor - Separation		15	15
Elevation - Visibility from Above		-5	
Buffers		10	6
- Visual		6	3.5
- Vegetation		1.75	1.75
- Fencing		1.75	1.75
- Berms		2.5	
- Noise		4	2.5
- Vegetation		1.25	1.25
- Fencing		1.25	1.25
- Berms		1.5	
Current Conditions	10%	10	5
Ex. Trail or Sidewalk & Used		10	
No Trail or Sidewalk, but Used		7	
No Trail or Sidewalk & Un-Used		5	5
Site Details	5%	5	2
Usable w/out Improvement		5	
Un-Usable w/ out Improvement		2	2
Public Opinion of APO**	25%	25	15
Support (75%+)		25	
Oppose (<25%)		0	
Mix - For vs. Against		10 - 15	15
Jurisdiction	5%	5	5
In City		5	5
In ETJ		3	
Total	100%	100	64

* Single Family Residential Property ** Adjacent Property Owners





CITY of CEDAR PARK - EVALUATION CRITERIA for HIKE & BIKE CORRIDORS
 Corridor Name: Little Elm Trail
 Type: Parkway Sidewalk
 Score: B
 Length: 3,815 ft.

Evaluation Criterion	Importance	Total Pts Available	Points
Connectivity	20%	20	6
Schools		6	6
Trail-to-Trail		6	6
Parks & Other Amenities		4	
Major Retail		2	
Major Employers		2	
Availability	15%	15	15
City Owned		15	15
Entity Owned		10	
Privately Owned			0
- Single Owner		6	
- Common Ownership (HOA)		4	
- Multiple Owners		2	
Proximity to SFR*	20%	20	18
Width of Corridor - Separation		15	15
Elevation - Visibility from Above		-5	
Buffers		10	3
- Visual		6	1.75
- Vegetation		1.75	
- Fencing		1.75	1.75
- Berms		2.5	
- Noise		4	1.25
- Vegetation		1.25	
- Fencing		1.25	1.25
- Berms		1.5	
Current Conditions	10%	10	5
Ex. Trail or Sidewalk & Used		10	
No Trail or Sidewalk, but Used		7	
No Trail or Sidewalk & Un-Used		5	5
Site Details	5%	5	2
Usable w/out Improvement		5	
Un-Usable w/ out Improvement		2	2
Public Opinion of APO**	25%	25	25
Support (75%+)		25	25
Oppose (<25%)		0	
Mix - For vs. Against		10 - 15	
Jurisdiction	5%	5	5
In City		5	5
In ETJ		3	
Total	100%	100	76

* Single Family Residential Property ** Adjacent Property Owners

Little Elm Trail

Little Elm Trail is another street that is part of the City's Transportation Master Plan. The currently is a ten foot meandering parkway sidewalk in the newly developed neighborhood. As the street is expanded, the ten foot parkway sidewalk should continue and maintain its current width.

Little Elm Trail provides a significant crossing into Twin Lakes Park and the existing Brushy Creek Regional Trail. When Little Elm Trail is extended to Bell Street, it is highly important that a safe pedestrian crossing be built that crosses Bell Street. This is one of the few intersections that will allow a connection from the existing Brushy Creek Regional Trail to the future trail system around Lakeline Village PUD Park.

Other gaps in the Little Elm Trail parkway sidewalk should be filled as the street is extended.



Orchard Falls Drive

This is a proposed sidewalk along Orchard Falls Drive which will connect the neighborhood to future Lakeline Village PUD Park. It is likely that this sidewalk will be constructed as a result of the development of this neighborhood.

CITY OF CEDAR PARK - EVALUATION CRITERIA for HIKE & BIKE CORRIDORS			
Corridor Name: Orchard Falls Drive		Score: B	
Type: Sidewalk		Length: 950 ft.	
Evaluation Criterion	Importance	Total Pts Available	Points
Connectivity	20%	20	10
Schools		6	
Trail-to-Trail		6	6
Parks & Other Amenities		4	4
Major Retail		2	
Major Employers		2	
Availability	15%	15	15
City Owned		15	15
Entity Owned		10	
Privately Owned			0
- Single Owner		6	
- Common Ownership (HOA)		4	
- Multiple Owners		2	
Proximity to SFR*	20%	20	18
Width of Corridor - Separation		15	15
Elevation - Visibility from Above		-5	
Buffers		10	3
- Visual		6	1.75
- Vegetation		1.75	
- Fencing		1.75	1.75
- Berms		2.5	
- Noise		4	1.25
- Vegetation		1.25	
- Fencing		1.25	1.25
- Berms		1.5	
Current Conditions	10%	10	10
Ex. Trail or Sidewalk & Used		10	10
No Trail or Sidewalk, but Used		7	
No Trail or Sidewalk & Un-Used		5	
Site Details	5%	5	5
Usable w/out Improvement		5	5
Un-Usable w/ out Improvement		2	
Public Opinion of APO**	25%	25	0
Support (75%+)		25	
Oppose (<25%)		0	
Mix - For vs. Against		10 - 15	
Jurisdiction	5%	5	5
In City		5	5
In ETJ		3	
Total	100%	100	63

* Single Family Residential Property ** Adjacent Property Owners





CITY OF CEDAR PARK - EVALUATION CRITERIA for HIKE & BIKE CORRIDORS			
Corridor Name: Eastern Developer Trails		Score: C	
Type: Developer Trails		Length: 2,715 ft.	
Evaluation Criterion	Importance	Total Pts Available	Points
Connectivity	20%	20	16
Schools		6	6
Trail-to-Trail		6	6
Parks & Other Amenities		4	4
Major Retail		2	
Major Employers		2	
Availability	15%	15	2
City Owned		15	
Entity Owned		10	
Privately Owned			2
- Single Owner		6	
- Common Ownership (HOA)		4	
- Multiple Owners		2	2
Proximity to SFR*	20%	20	18
Width of Corridor - Separation		15	15
Elevation - Visibility from Above		-5	
Buffers		10	3
- Visual		6	1.75
- Vegetation		1.75	1.75
- Fencing		1.75	
- Berms		2.5	
- Noise		4	1.25
- Vegetation		1.25	1.25
- Fencing		1.25	
- Berms		1.5	
Current Conditions	10%	10	5
Ex. Trail or Sidewalk & Used		10	
No Trail or Sidewalk, but Used		7	
No Trail or Sidewalk & Un-Used		5	5
Site Details	5%	5	2
Usable w/out Improvement		5	
Un-Usable w/ out Improvement		2	2
Public Opinion of APO**	25%	25	0
Support (75%+)		25	
Oppose (<25%)		0	
Mix - For vs. Against		10 - 15	
Jurisdiction	5%	5	5
In City		5	5
In ETJ		3	
Total	100%	100	48

* Single Family Residential Property ** Adjacent Property Owners

Eastern Developer Trails

Developer trails are proposed in the eastern portion of this sector to connect the proposed collector street to Bell St. As future development occurs in this area, trails should be built to ensure an interconnected system.



Proposed Collector Trail

There is a proposed collector street in the City's future transportation plan that will connect Cypress Creek Road to Lake Cedar Park. A trail should be built along side this proposed street because of the great connection it makes. This trail will have the potential to connect the neighborhoods and multi-family apartment complexes north of Cypress Creek Road to the Lakeline Village PUD Park and possibly to the existing Brushy Creek Regional Trail via Little Elm Trail.

CITY OF CEDAR PARK - EVALUATION CRITERIA for HIKE & BIKE CORRIDORS			
Corridor Name: Proposed Collector Trail		Score: C	
Type: Trail		Length: 2,535 ft.	
Evaluation Criterion	Importance	Total Pts Available	Points
Connectivity	20%	20	10
Schools		6	6
Trail-to-Trail		6	6
Parks & Other Amenities		4	4
Major Retail		2	2
Major Employers		2	2
Availability	15%	15	2
City Owned		15	2
Entity Owned		10	2
Privately Owned			2
- Single Owner		6	2
- Common Ownership (HOA)		4	2
- Multiple Owners		2	2
Proximity to SFR*	20%	20	18
Width of Corridor - Separation		15	15
Elevation - Visibility from Above		-5	3
Buffers		10	3
- Visual		6	1.75
- Vegetation		1.75	1.75
- Fencing		1.75	
- Berms		2.5	
- Noise		4	1.25
- Vegetation		1.25	1.25
- Fencing		1.25	
- Berms		1.5	
Current Conditions	10%	10	5
Ex. Trail or Sidewalk & Used		10	5
No Trail or Sidewalk, but Used		7	5
No Trail or Sidewalk & Un-Used		5	5
Site Details	5%	5	2
Usable w/out Improvement		5	2
Un-Usable w/ out Improvement		2	2
Public Opinion of APO**	25%	25	0
Support (75%+)		25	0
Oppose (<25%)		0	0
Mix - For vs. Against		10 - 15	0
Jurisdiction	5%	5	5
In City		5	5
In ETJ		3	5
Total	100%	100	42

* Single Family Residential Property ** Adjacent Property Owners





CITY OF CEDAR PARK - EVALUATION CRITERIA for HIKE & BIKE CORRIDORS			
Corridor Name: Lakeline Blvd.		Score: A	
Type: Parkway Sidewalk		Length: 8,160 ft.	
Evaluation Criterion	Importance	Total Pts Available	Points
Connectivity	20%	20	4
Schools		6	
Trail-to-Trail		6	
Parks & Other Amenities		4	4
Major Retail		2	
Major Employers		2	
Availability	15%	15	15
City Owned		15	15
Entity Owned		10	
Privately Owned			0
- Single Owner		6	
- Common Ownership (HOA)		4	
- Multiple Owners		2	
Proximity to SFR*	20%	20	18
Width of Corridor - Separation		15	15
Elevation - Visibility from Above		-5	
Buffers		10	3
- Visual		6	1.75
- Vegetation		1.75	1.75
- Fencing		1.75	
- Berms		2.5	
- Noise		4	1.25
- Vegetation		1.25	1.25
- Fencing		1.25	
- Berms		1.5	
Current Conditions	10%	10	10
Ex. Trail or Sidewalk & Used		10	10
No Trail or Sidewalk, but Used		7	
No Trail or Sidewalk & Un-Used		5	
Site Details	5%	5	5
Usable w/out Improvement		5	5
Un-Usable w/ out Improvement		2	
Public Opinion of APO**	25%	25	25
Support (75%+)		25	25
Oppose (<25%)		0	
Mix - For vs. Against		10 - 15	
Jurisdiction	5%	5	5
In City		5	5
In ETJ		3	
Total	100%	100	82

* Single Family Residential Property ** Adjacent Property Owners

Lakeline Blvd.

This plan proposes that one of the sidewalks along Lakeline Blvd. be widened into a parkway sidewalk of at least eight feet. Lakeline Blvd. is a major arterial that connects a significant portion of the City. The sidewalk along the street currently has a nice trail setting with a meandering sidewalk and gazebos every few hundred feet in some sections. However, it should be widened so that it is recognized as a trail corridor, and is able to be used by multiple users.

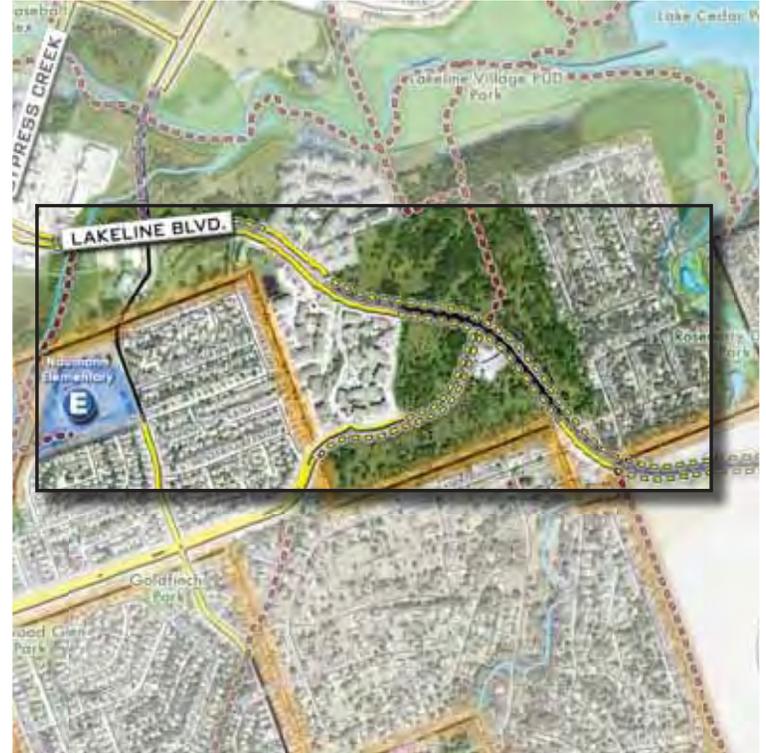


South Lakeline Blvd.

The sidewalks along Lakeline Blvd. south of Cypress Creek Road are sporadic. The sidewalk should be expanded on both sides of the street so that it is continuous. The City of Cedar Park should work with the City of Austin to extend the sidewalks along Lakeline Blvd. until it reaches the Cap Metro Rail Station. Lakeline Blvd. provides a significant connection to this public transit destination.

CITY OF CEDAR PARK - EVALUATION CRITERIA for HIKE & BIKE CORRIDORS			
Corridor Name: South Lakeline Blvd.			
Type: Sidewalk			
Score: A			
Length: 8,500 ft.			
Evaluation Criterion	Importance	Total Pts Available	Points
Connectivity	20%	20	6
Schools		6	
Trail-to-Trail		6	
Parks & Other Amenities		4	4
Major Retail		2	2
Major Employers		2	
Availability	15%	15	15
City Owned		15	15
Entity Owned		10	
Privately Owned			0
- Single Owner		6	
- Common Ownership (HOA)		4	
- Multiple Owners		2	
Proximity to SFR*	20%	20	18
Width of Corridor - Separation		15	15
Elevation - Visibility from Above		-5	
Buffers		10	3
- Visual		6	1.75
- Vegetation		1.75	1.75
- Fencing		1.75	
- Berms		2.5	
- Noise		4	1.25
- Vegetation		1.25	1.25
- Fencing		1.25	
- Berms		1.5	
Current Conditions	10%	10	7
Ex. Trail or Sidewalk & Used		10	
No Trail or Sidewalk, but Used		7	7
No Trail or Sidewalk & Un-Used		5	
Site Details	5%	5	5
Usable w/out Improvement		5	5
Un-Usable w/ out Improvement		2	
Public Opinion of APO**	25%	25	25
Support (75%+)		25	25
Oppose (<25%)		0	
Mix - For vs. Against		10 - 15	
Jurisdiction	5%	5	5
In City		5	5
In ETJ		3	
Total	100%	100	81

* Single Family Residential Property ** Adjacent Property Owners





CITY OF CEDAR PARK - EVALUATION CRITERIA for HIKE & BIKE CORRIDORS
 Corridor Name: School Drainage
 Type: Trail
 Score: B
 Length: 5,660 ft.

Evaluation Criterion	Importance	Total Pts Available	Points
Connectivity	20%	20	10
Schools		6	6
Trail-to-Trail		6	
Parks & Other Amenities		4	4
Major Retail		2	
Major Employers		2	
Availability	15%	15	10
City Owned		15	
Entity Owned		10	10
Privately Owned			0
- Single Owner		6	
- Common Ownership (HOA)		4	
- Multiple Owners		2	
Proximity to SFR*	20%	20	18
Width of Corridor - Separation		15	15
Elevation - Visibility from Above		-5	
Buffers		10	3
- Visual		6	1.75
- Vegetation		1.75	1.75
- Fencing		1.75	
- Berms		2.5	
- Noise		4	1.25
- Vegetation		1.25	1.25
- Fencing		1.25	
- Berms		1.5	
Current Conditions	10%	10	5
Ex. Trail or Sidewalk & Used		10	
No Trail or Sidewalk, but Used		7	
No Trail or Sidewalk & Un-Used		5	5
Site Details	5%	5	2
Usable w/out Improvement		5	
Un-Usable w/ out Improvement		2	2
Public Opinion of APO**	25%	25	15
Support (75%+)		25	
Oppose (<25%)		0	
Mix - For vs. Against		10 - 15	15
Jurisdiction	5%	5	5
In City		5	5
In ETJ		3	
Total	100%	100	65

* Single Family Residential Property ** Adjacent Property Owners

School Drainage

There is a drainage corridor that runs behind Cedar Park Middle School and Naumann Elementary School. This drainage corridor connects Sun Chase Blvd. to Lakeline Blvd. Developing a trail along this corridor will provide direct access to the schools from the neighborhoods surrounding them. Currently, the only access point to the elementary school is off of Little Elm Trail. If a trail were developed along the drainage corridor, then students and their parents could walk or bike to the school without being routed through the neighborhood.



Old Mill Road (ETJ Limit to Lakeline Blvd.)

The sidewalk along the south side of Old Mill Road currently ends once it reaches the ETJ limit. The sidewalk should be extended along the street until it reaches Lakeline Blvd.

CITY OF CEDAR PARK - EVALUATION CRITERIA for HIKE & BIKE CORRIDORS				
Corridor Name: Old Mill Road (ETJ Limit to Lakeline Blvd.)			Score: C	
Type: Sidewalk			Length: 3,000 ft.	
Evaluation Criterion	Importance	Total Pts Available	Points	
Connectivity	20%	20	0	
Schools		6		
Trail-to-Trail		6		
Parks & Other Amenities		4		
Major Retail		2		
Major Employers		2		
Availability	15%	15	15	
City Owned		15	15	
Entity Owned		10		
Privately Owned			0	
- Single Owner		6		
- Common Ownership (HOA)		4		
- Multiple Owners		2		
Proximity to SFR*	20%	20	18	
Width of Corridor - Separation		15	15	
Elevation - Visibility from Above		-5		
Buffers		10	3	
- Visual		6	1.75	
- Vegetation		1.75	1.75	
- Fencing		1.75		
- Berms		2.5		
- Noise		4	1.25	
- Vegetation		1.25	1.25	
- Fencing		1.25		
- Berms		1.5		
Current Conditions	10%	10	5	
Ex. Trail or Sidewalk & Used		10		
No Trail or Sidewalk, but Used		7		
No Trail or Sidewalk & Un-Used		5	5	
Site Details	5%	5	5	
Usable w/out Improvement		5	5	
Un-Usable w/ out Improvement		2		
Public Opinion of APO**	25%	25	0	
Support (75%+)		25		
Oppose (<25%)		0		
Mix - For vs. Against		10 - 15		
Jurisdiction	5%	5	5	
In City		5	5	
In ETJ		3		
Total	100%	100	48	

* Single Family Residential Property ** Adjacent Property Owners





CITY OF CEDAR PARK - EVALUATION CRITERIA for HIKE & BIKE CORRIDORS
 Corridor Name: Powerline Corridor (El Salido to Old Mill)
 Type: Trail
 Length: 4,675 ft.
 Score: D

Evaluation Criterion	Importance	Total Pts Available	Points
Connectivity	20%	20	4
Schools		6	
Trail-to-Trail		6	
Parks & Other Amenities		4	4
Major Retail		2	
Major Employers		2	
Availability	15%	15	2
City Owned		15	
Entity Owned		10	
Privately Owned			2
- Single Owner		6	
- Common Ownership (HOA)		4	
- Multiple Owners		2	2
Proximity to SFR*	20%	20	16
Width of Corridor - Separation		15	15
Elevation - Visibility from Above		-5	-5
Buffers		10	6
- Visual		6	3.5
- Vegetation		1.75	1.75
- Fencing		1.75	1.75
- Berms		2.5	
- Noise		4	2.5
- Vegetation		1.25	1.25
- Fencing		1.25	1.25
- Berms		1.5	
Current Conditions	10%	10	7
Ex. Trail or Sidewalk & Used		10	
No Trail or Sidewalk, but Used		7	7
No Trail or Sidewalk & Un-Used		5	
Site Details	5%	5	5
Usable w/out Improvement		5	5
Un-Usable w/ out Improvement		2	
Public Opinion of APO**	25%	25	0
Support (75%+)		25	
Oppose (<25%)		0	
Mix - For vs. Against		10 - 15	
Jurisdiction	5%	5	4
In City		5	4
In ETJ		3	
Total	100%	100	38

* Single Family Residential Property ** Adjacent Property Owners

Powerline Corridor (El Salido to Old Mill Rd.)

The powerline corridor in the far southwestern portion of the City has great potential for a future trail. One foreseeable problem is that the home owners' property extends to the centerline of the easement. This will make construction difficult because an agreement must be reached with every property owner.



ETJ Limit Trail

A trail is proposed along the ETJ limit near Lakeline Mall. This trail may be difficult because it appears that the home owners' property comes all the way to Cedar Park's ETJ. If that is the case, then there is not space to develop a trail. Therefore, the City should work with the City of Austin to construct a trail which can connect to the mall and RM 620 to Lakeline Blvd.

CITY OF CEDAR PARK - EVALUATION CRITERIA for HIKE & BIKE CORRIDORS			
Corridor Name: ETJ Limit Trail		Score: D	
Type: Trail		Length: 4,355 ft.	
Evaluation Criterion	Importance	Total Pts Available	Points
Connectivity	20%	20	4
Schools		6	
Trail-to-Trail		6	
Parks & Other Amenities		4	
Major Retail		2	2
Major Employers		2	2
Availability	15%	15	15
City Owned		15	15
Entity Owned		10	
Privately Owned			0
- Single Owner		6	
- Common Ownership (HOA)		4	
- Multiple Owners		2	
Proximity to SFR*	20%	20	6
Width of Corridor - Separation		15	
Elevation - Visibility from Above		-5	
Buffers		10	6
- Visual		6	3.5
- Vegetation		1.75	1.75
- Fencing		1.75	1.75
- Berms		2.5	
- Noise		4	2.5
- Vegetation		1.25	1.25
- Fencing		1.25	1.25
- Berms		1.5	
Current Conditions	10%	10	5
Ex. Trail or Sidewalk & Used		10	
No Trail or Sidewalk, but Used		7	
No Trail or Sidewalk & Un-Used		5	5
Site Details	5%	5	2
Usable w/out Improvement		5	
Un-Usable w/ out Improvement		2	2
Public Opinion of APO**	25%	25	0
Support (75%+)		25	
Oppose (<25%)		0	
Mix - For vs. Against		10 - 15	
Jurisdiction	5%	5	3
In City		5	
In ETJ		3	3
Total	100%	100	35

* Single Family Residential Property ** Adjacent Property Owners





CITY of CEDAR PARK - EVALUATION CRITERIA for HIKE & BIKE CORRIDORS

Corridor Name: RM 620 Trail
 Type: Sidewalk
 Score: B
 Length: 4,665 ft.

Evaluation Criterion	Importance	Total Pts Available	Points
Connectivity	20%	20	10
Schools		6	6
Trail-to-Trail		6	
Parks & Other Amenities		4	
Major Retail		2	2
Major Employers		2	2
Availability	15%	15	15
City Owned		15	15
Entity Owned		10	
Privately Owned			0
- Single Owner		6	
- Common Ownership (HOA)		4	
- Multiple Owners		2	
Proximity to SFR*	20%	20	3
Width of Corridor - Separation		15	
Elevation - Visibility from Above		-5	
Buffers		10	3
- Visual		6	1.75
- Vegetation		1.75	1.75
- Fencing		1.75	
- Berms		2.5	
- Noise		4	1.25
- Vegetation		1.25	1.25
- Fencing		1.25	
- Berms		1.5	
Current Conditions	10%	10	5
Ex. Trail or Sidewalk & Used		10	
No Trail or Sidewalk, but Used		7	
No Trail or Sidewalk & Un-Used		5	5
Site Details	5%	5	2
Usable w/out Improvement		5	
Un-Usable w/ out Improvement		2	2
Public Opinion of APO**	25%	25	25
Support (75%+)		25	25
Oppose (<25%)		0	
Mix - For vs. Against		10 - 15	
Jurisdiction	5%	5	5
In City		5	5
In ETJ		3	
Total	100%	100	65

* Single Family Residential Property ** Adjacent Property Owners

RM 620 Trail

RM 620 is a major corridor and a busy vehicular street. A continuous sidewalk should be built on the north side of the street, in Cedar Park's jurisdiction. This sidewalk should be wide enough to accommodate both pedestrians and bicyclists. It will provide a strong connection from the Twin Creeks neighborhood to the Lakeline Mall.



Volente Road

The construction of a sidewalk along Volente Road will connect the Twin Creeks neighborhood and golf course to RM 620 and eventually Lakeline Mall. The sidewalk should extend from RM 620 to Twin Creeks Club Dr. The sidewalk should also connect to the Twin Creeks Historic Park Trail.

CITY OF CEDAR PARK - EVALUATION CRITERIA for HIKE & BIKE CORRIDORS			
Corridor Name: Volente Road		Score: C	
Type: Sidewalk		Length: 13,495 ft.	
Evaluation Criterion	Importance	Total Pts Available	Points
Connectivity	20%	20	6
Schools		6	
Trail-to-Trail		6	
Parks & Other Amenities		4	4
Major Retail		2	
Major Employers		2	2
Availability	15%	15	15
City Owned		15	15
Entity Owned		10	
Privately Owned			0
- Single Owner		6	
- Common Ownership (HOA)		4	
- Multiple Owners		2	
Proximity to SFR*	20%	20	21
Width of Corridor - Separation		15	15
Elevation - Visibility from Above		-5	
Buffers		10	6
- Visual		6	3.5
- Vegetation		1.75	1.75
- Fencing		1.75	1.75
- Berms		2.5	
- Noise		4	2.5
- Vegetation		1.25	1.25
- Fencing		1.25	1.25
- Berms		1.5	
Current Conditions	10%	10	5
Ex. Trail or Sidewalk & Used		10	
No Trail or Sidewalk, but Used		7	
No Trail or Sidewalk & Un-Used		5	5
Site Details	5%	5	5
Usable w/out Improvement		5	5
Un-Usable w/ out Improvement		2	
Public Opinion of APO**	25%	25	0
Support (75%+)		25	
Oppose (<25%)		0	
Mix - For vs. Against		10 - 15	
Jurisdiction	5%	5	4
In City		5	4
In ETJ		3	
Total	100%	100	56

* Single Family Residential Property ** Adjacent Property Owners





CITY OF CEDAR PARK - EVALUATION CRITERIA for HIKE & BIKE CORRIDORS
 Corridor Name: Twin Creeks Historic Park Trail
 Type: Trail
 Length: 5,900 ft.
 Score: C

Evaluation Criterion	Importance	Total Pts Available	Points
Connectivity	20%	20	16
Schools		6	6
Trail-to-Trail		6	6
Parks & Other Amenities		4	4
Major Retail		2	
Major Employers		2	
Availability	15%	15	4
City Owned		15	
Entity Owned		10	
Privately Owned			4
- Single Owner		6	
- Common Ownership (HOA)		4	4
- Multiple Owners		2	
Proximity to SFR*	20%	20	25
Width of Corridor - Separation		15	15
Elevation - Visibility from Above		-5	
Buffers		10	10
- Visual		6	6
- Vegetation		1.75	1.75
- Fencing		1.75	1.75
- Berms		2.5	2.5
- Noise		4	4
- Vegetation		1.25	1.25
- Fencing		1.25	1.25
- Berms		1.5	1.5
Current Conditions	10%	10	7
Ex. Trail or Sidewalk & Used		10	
No Trail or Sidewalk, but Used		7	7
No Trail or Sidewalk & Un-Used		5	
Site Details	5%	5	5
Usable w/out Improvement		5	5
Un-Usable w/ out Improvement		2	
Public Opinion of APO**	25%	25	0
Support (75%+)		25	
Oppose (<25%)		0	
Mix - For vs. Against		10 - 15	
Jurisdiction	5%	5	3
In City		5	
In ETJ		3	3
Total	100%	100	60

* Single Family Residential Property ** Adjacent Property Owners

Twin Creeks Historic Park Trail

The master plan completed for Twin Creeks Historic Park proposes trails throughout the park, from Volente Rd. to Zennor Ct. It is proposed to connect to the existing sidewalk once the trail reaches Zennor Ct. It will then connect to Twin Creeks Club Dr. If the trail were to continue off street, then it would be required to cross over private property until it reaches Anderson Mill Rd. Therefore, the segment of the trail that would be on private property is a proposed developer trail.

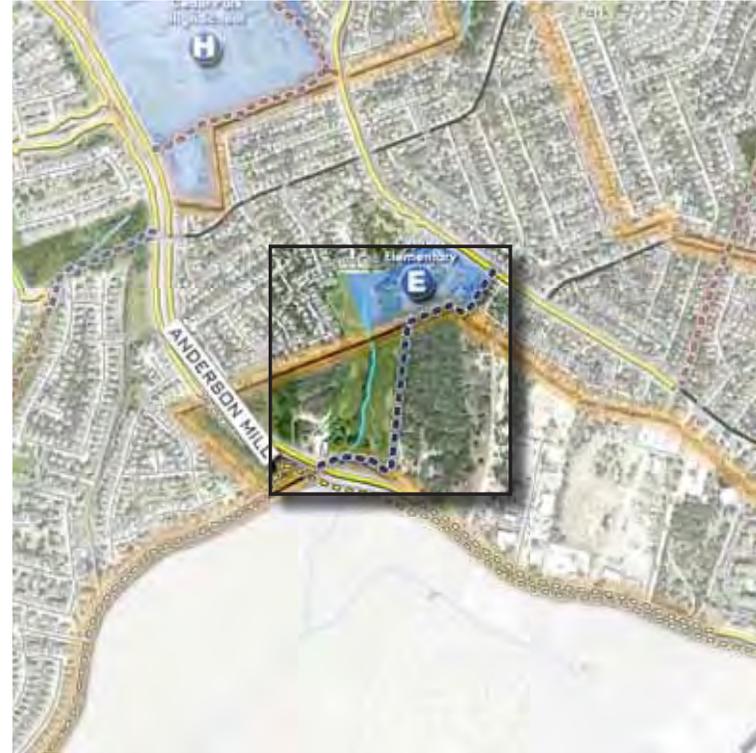


Sewer Main Corridor

A trail is proposed over the sewer main easement which connects El Salido Pkwy. to Anderson Mill Rd. The proposed trail connects to Cypress Elementary School.

CITY OF CEDAR PARK - EVALUATION CRITERIA for HIKE & BIKE CORRIDORS				
Corridor Name: Sewer Main Corridor		Score: C		
Type: Developer Trail		Length: 11,690 ft.		
Evaluation Criterion	Importance	Total Pts Available	Points	
Connectivity	20%	20	8	
Schools		6	6	
Trail-to-Trail		6		
Parks & Other Amenities		4		
Major Retail		2		
Major Employers		2	2	
Availability	15%	15	6	
City Owned		15		
Entity Owned		10		
Privately Owned			6	
- Single Owner		6	6	
- Common Ownership (HOA)		4		
- Multiple Owners		2		
Proximity to SFR*	20%	20	18	
Width of Corridor - Separation		15	15	
Elevation - Visibility from Above		-5		
Buffers		10	3	
- Visual		6	1.75	
- Vegetation		1.75	1.75	
- Fencing		1.75		
- Berms		2.5		
- Noise		4	1.25	
- Vegetation		1.25	1.25	
- Fencing		1.25		
- Berms		1.5		
Current Conditions	10%	10	5	
Ex. Trail or Sidewalk & Used		10		
No Trail or Sidewalk, but Used		7		
No Trail or Sidewalk & Un-Used		5	5	
Site Details	5%	5	2	
Usable w/out Improvement		5		
Un-Usable w/ out Improvement		2	2	
Public Opinion of APO**	25%	25	0	
Support (75%+)		25		
Oppose (<25%)		0		
Mix - For vs. Against		10 - 15		
Jurisdiction	5%	5	5	
In City		5	5	
In ETJ		3		
Total	100%	100	44	

* Single Family Residential Property ** Adjacent Property Owners





CITY of CEDAR PARK - EVALUATION CRITERIA for HIKE & BIKE CORRIDORS
 Corridor Name: Cedar Park High School Trail
 Type: Trail
 Score: C
 Length: 3,830 ft.

Evaluation Criterion	Importance	Total Pts Available	Points
Connectivity	20%	20	12
Schools		6	6
Trail-to-Trail		6	6
Parks & Other Amenities		4	
Major Retail		2	
Major Employers		2	
Availability	15%	15	10
City Owned		15	
Entity Owned		10	10
Privately Owned			0
- Single Owner		6	
- Common Ownership (HOA)		4	
- Multiple Owners		2	
Proximity to SFR*	20%	20	21
Width of Corridor - Separation		15	15
Elevation - Visibility from Above		-5	
Buffers		10	6
- Visual		6	3.5
- Vegetation		1.75	1.75
- Fencing		1.75	1.75
- Berms		2.5	
- Noise		4	2.5
- Vegetation		1.25	1.25
- Fencing		1.25	1.25
- Berms		1.5	
Current Conditions	10%	10	5
Ex. Trail or Sidewalk & Used		10	
No Trail or Sidewalk, but Used		7	
No Trail or Sidewalk & Un-Used		5	5
Site Details	5%	5	5
Usable w/out Improvement		5	5
Un-Usable w/ out Improvement		2	
Public Opinion of APO**	25%	25	0
Support (75%+)		25	
Oppose (<25%)		0	
Mix - For vs. Against		10 - 15	
Jurisdiction	5%	5	5
In City		5	5
In ETJ		3	
Total	100%	100	58

* Single Family Residential Property ** Adjacent Property Owners

Cedar Park High School Trail

This proposed trail runs along the outside property line of Cedar Park High School. This trail can connect the high school to the surrounding neighborhoods without the students having to walk along the busy arterial streets of Cypress Creek Rd. and Anderson Mill Rd.



Summit Christian Academy Trail

A trail is proposed to connect Summit Christian Academy to Heather Dr. and Elizabeth Milburn Park. Currently the only access point to the private school is a long driveway off Cypress Creek Rd. This proposed trail connects to the school driveway so there is pedestrian access for the students and school employees to the community park.

CITY OF CEDAR PARK - EVALUATION CRITERIA for HIKE & BIKE CORRIDORS			
Corridor Name: Summit Christian Academy Trail		Score: C	
Type: Trail		Length: 890 ft.	
Evaluation Criterion	Importance	Total Pts Available	Points
Connectivity	20%	20	6
Schools		6	6
Trail-to-Trail		6	
Parks & Other Amenities		4	
Major Retail		2	
Major Employers		2	
Availability	15%	15	10
City Owned		15	
Entity Owned		10	10
Privately Owned			0
- Single Owner		6	
- Common Ownership (HOA)		4	
- Multiple Owners		2	
Proximity to SFR*	20%	20	21
Width of Corridor - Separation		15	15
Elevation - Visibility from Above		-5	
Buffers		10	6
- Visual		6	3.5
- Vegetation		1.75	1.75
- Fencing		1.75	1.75
- Berms		2.5	
- Noise		4	2.5
- Vegetation		1.25	1.25
- Fencing		1.25	1.25
- Berms		1.5	
Current Conditions	10%	10	5
Ex. Trail or Sidewalk & Used		10	
No Trail or Sidewalk, but Used		7	
No Trail or Sidewalk & Un-Used		5	5
Site Details	5%	5	2
Usable w/out Improvement		5	
Un-Usable w/ out Improvement		2	2
Public Opinion of APO**	25%	25	0
Support (75%+)		25	
Oppose (<25%)		0	
Mix - For vs. Against		10 - 15	
Jurisdiction	5%	5	5
In City		5	5
In ETJ		3	
Total	100%	100	49

* Single Family Residential Property ** Adjacent Property Owners





Bell Blvd. (Park St. north to City Limits)

Bell Street is part of the City's Transportation Master Plan. When Bell Street is expanded or improved, at least a six foot meandering sidewalk must be placed on one side of the street. This Master Plan recommends the sidewalk be a parkway sidewalk of at least eight feet in width. This allows for multiple users to be on the sidewalk comfortably. It can be expected that both pedestrians and bicyclists will be using the parkway sidewalk.

Bell Street is a major arterial and connects the entire City by running north to south. There are several major destinations along the street such as retail, restaurants, and City Hall. An eight to ten foot wide parkway sidewalk is more practical for this type of street than a six foot wide sidewalk.

CITY OF CEDAR PARK - EVALUATION CRITERIA for HIKE & BIKE CORRIDORS			
Corridor Name: Bell St. (Park St. to city limits)		Score: B	
Type: Parkway Sidewalk		Length: 15,185 ft.	
Evaluation Criterion	Importance	Total Pts Available	Points
Connectivity	20%	20	4
Schools		6	
Trail-to-Trail		6	
Parks & Other Amenities		4	
Major Retail		2	2
Major Employers		2	2
Availability	15%	15	15
City Owned		15	15
Entity Owned		10	
Privately Owned			0
- Single Owner		6	
- Common Ownership (HOA)		4	
- Multiple Owners		2	
Proximity to SFR*	20%	20	15
Width of Corridor - Separation		15	15
Elevation - Visibility from Above		-5	
Buffers		10	0
- Visual		6	0
- Vegetation		1.75	
- Fencing		1.75	
- Berms		2.5	
- Noise		4	0
- Vegetation		1.25	
- Fencing		1.25	
- Berms		1.5	
Current Conditions	10%	10	10
Ex. Trail or Sidewalk & Used		10	10
No Trail or Sidewalk, but Used		7	
No Trail or Sidewalk & Un-Used		5	
Site Details	5%	5	2
Usable w/out Improvement		5	
Un-Usable w/ out Improvement		2	2
Public Opinion of APO**	25%	25	25
Support (75%+)		25	25
Oppose (<25%)		0	
Mix - For vs. Against		10 - 15	
Jurisdiction	5%	5	5
In City		5	5
In ETJ		3	
Total	100%	100	76

* Single Family Residential Property ** Adjacent Property Owners





CITY OF CEDAR PARK - EVALUATION CRITERIA for HIKE & BIKE CORRIDORS
 Corridor Name: Cap Metro Rail Trail (Park St to city limits) Score: A
 Type: Trail Length: 15,075 ft.

Evaluation Criterion	Importance	Total Pts Available	Points
Connectivity	20%	20	14
Schools		6	6
Trail-to-Trail		6	6
Parks & Other Amenities		4	4
Major Retail		2	2
Major Employers		2	2
Availability	15%	15	10
City Owned		15	10
Entity Owned		10	10
Privately Owned			0
- Single Owner		6	6
- Common Ownership (HOA)		4	4
- Multiple Owners		2	2
Proximity to SFR*	20%	20	22
Width of Corridor - Separation		15	15
Elevation - Visibility from Above		-5	0
Buffers		10	7
- Visual		6	4.25
- Vegetation		1.75	1.75
- Fencing		1.75	1.75
- Berms		2.5	2.5
- Noise		4	2.75
- Vegetation		1.25	1.25
- Fencing		1.25	1.25
- Berms		1.5	1.5
Current Conditions	10%	10	5
Ex. Trail or Sidewalk & Used		10	10
No Trail or Sidewalk, but Used		7	7
No Trail or Sidewalk & Un-Used		5	5
Site Details	5%	5	2
Usable w/out Improvement		5	5
Un-Usable w/ out Improvement		2	2
Public Opinion of APO**	25%	25	25
Support (75%+)		25	25
Oppose (<25%)		0	0
Mix - For vs. Against		10 - 15	10 - 15
Jurisdiction	5%	5	5
In City		5	5
In ETJ		3	3
Total	100%	100	83

* Single Family Residential Property ** Adjacent Property Owners

Cap Metro Rail Trail (Park St. north to City Limits)

During the public input process, residents expressed an interest in a trail along the Cap Metro Rail Line. The railroad extends through the entire City, and passes by several destinations. The Library, City Hall, and the existing Brushy Creek Regional Trail are all adjacent to the railroad.

The trail might prove to be difficult in some areas. The distance between the trail and the rail tracks needs to be at least 10 feet for low speed trails, and 35 feet for high speed trails (25 feet is allow if there is protective landscaping or fencing). The entire right-of-way width must be 50 feet for high speed trails. In many places, the corridor is too narrow to accommodate both the railroad tracks and a trail.

The proposed trail is included in this Master Plan in the chance that the railroad tracks are ever abandoned by Cap Metro. Abandoned railroad corridors have great potential to be converted into trails. If at any point in the future the tracks are abandoned, then the City should seek to build a trail on the rail property.





Block House Creek (ETJ)

The Block House Creek passes through the Block House Creek MUD. Developing trails along this greenbelt can connect the entire MUD to the park, HOA swimming pool, and existing trails at the entrance of the neighborhood. It can also connect residents to Block House Creek Elementary school and provide a safe route to the school.

Block House Creek collects into a lake west of 183A. The creek crosses through several private properties, and the lake is half in Cedar Park's jurisdiction and half in Leander's jurisdiction. Developing trails along Block House Creek greenbelt west of 183A could be difficult and requires cooperation of several landowners. The west half of the proposed trails should then be considered as a long term potential.

CITY of CEDAR PARK - EVALUATION CRITERIA for HIKE & BIKE CORRIDORS			
Corridor Name: Block House Creek		Score: B	
Type: Trail		Length: 25,685 ft.	
Evaluation Criterion	Importance	Total Pts Available	Points
Connectivity	20%	20	16
Schools		6	6
Trail-to-Trail		6	6
Parks & Other Amenities		4	4
Major Retail		2	
Major Employers		2	
Availability	15%	15	4
City Owned		15	
Entity Owned		10	
Privately Owned			4
- Single Owner		6	
- Common Ownership (HOA)		4	4
- Multiple Owners		2	
Proximity to SFR*	20%	20	16
Width of Corridor - Separation		15	15
Elevation - Visibility from Above		-5	-5
Buffers		10	6
- Visual		6	3.5
- Vegetation		1.75	1.75
- Fencing		1.75	1.75
- Berms		2.5	
- Noise		4	2.5
- Vegetation		1.25	1.25
- Fencing		1.25	1.25
- Berms		1.5	
Current Conditions	10%	10	7
Ex. Trail or Sidewalk & Used		10	
No Trail or Sidewalk, but Used		7	7
No Trail or Sidewalk & Un-Used		5	
Site Details	5%	5	5
Usable w/out Improvement		5	5
Un-Usable w/ out Improvement		2	
Public Opinion of APO**	25%	25	25
Support (75%+)		25	25
Oppose (<25%)		0	
Mix - For vs. Against		10 - 15	
Jurisdiction	5%	5	3
In City		5	
In ETJ		3	3
Total	100%	100	76

* Single Family Residential Property ** Adjacent Property Owners





CITY of CEDAR PARK - EVALUATION CRITERIA for HIKE & BIKE CORRIDORS			
Corridor Name: Cedar Park Center Trails		Score: B	
Type: Developer Trails		Length: 5,365 ft.	
Evaluation Criterion	Importance	Total Pts Available	Points
Connectivity	20%	20	12
Schools		6	
Trail-to-Trail		6	6
Parks & Other Amenities		4	4
Major Retail		2	
Major Employers		2	2
Availability	15%	15	6
City Owned		15	
Entity Owned		10	
Privately Owned			6
- Single Owner		6	6
- Common Ownership (HOA)		4	
- Multiple Owners		2	
Proximity to SFR*	20%	20	21
Width of Corridor - Separation		15	15
Elevation - Visibility from Above		-5	
Buffers		10	6
- Visual		6	3.5
- Vegetation		1.75	1.75
- Fencing		1.75	1.75
- Berms		2.5	
- Noise		4	2.5
- Vegetation		1.25	1.25
- Fencing		1.25	1.25
- Berms		1.5	
Current Conditions	10%	10	7
Ex. Trail or Sidewalk & Used		10	
No Trail or Sidewalk, but Used		7	7
No Trail or Sidewalk & Un-Used		5	
Site Details	5%	5	2
Usable w/out Improvement		5	
Un-Usable w/ out Improvement		2	2
Public Opinion of APO**	25%	25	25
Support (75%+)		25	25
Oppose (<25%)		0	
Mix - For vs. Against		10 - 15	
Jurisdiction	5%	5	5
In City		5	5
In ETJ		3	
Total	100%	100	78

* Single Family Residential Property ** Adjacent Property Owners

Event Center Trails

Developer trails are proposed around the Cedar Park Center property. This is to ensure connectivity to the Block House Creek neighborhood and the Town Center neighborhood. The Cedar Park Center is a major destination in the City, so residents should be given the option to either walk or bike to it instead of being forced to drive. These trails will give them that opportunity.

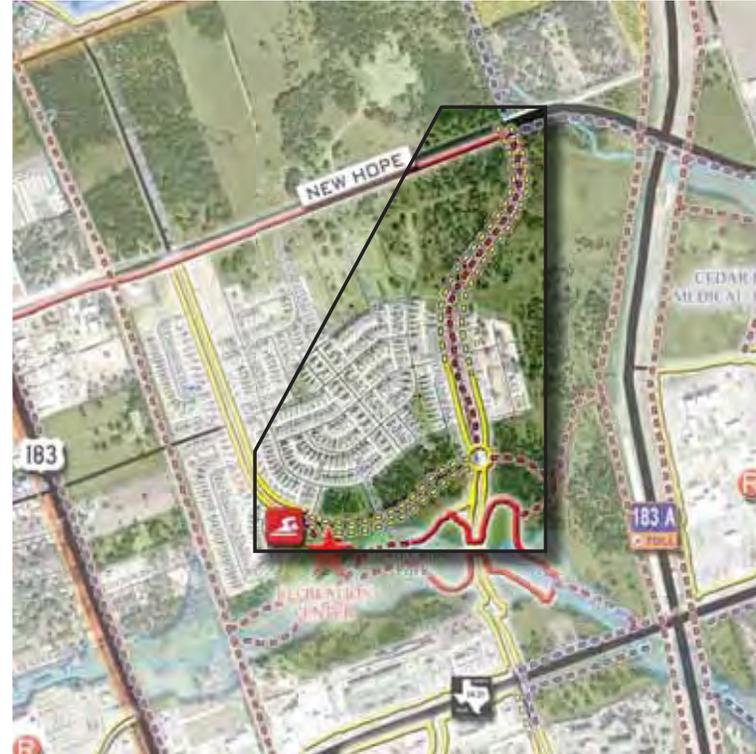


Town Center Sidewalks

As development of the Town Center residential properties is continued, sidewalks should be added to all streets. The developer of the Town Center has placed sidewalks on all existing streets, so the City should monitor to make sure sidewalk are added along Discovery Blvd. and Main St.

CITY OF CEDAR PARK - EVALUATION CRITERIA for HIKE & BIKE CORRIDORS			
Corridor Name: Town Center Sidewalks		Score: A	
Type: Sidewalk		Length: 7,900 ft.	
Evaluation Criterion	Importance	Total Pts Available	Points
Connectivity	20%	20	12
Schools		6	6
Trail-to-Trail		6	6
Parks & Other Amenities		4	4
Major Retail		2	2
Major Employers		2	2
Availability	15%	15	15
City Owned		15	15
Entity Owned		10	10
Privately Owned			0
- Single Owner		6	6
- Common Ownership (HOA)		4	4
- Multiple Owners		2	2
Proximity to SFR*	20%	20	18
Width of Corridor - Separation		15	15
Elevation - Visibility from Above		-5	0
Buffers		10	3
- Visual		6	1.75
- Vegetation		1.75	1.75
- Fencing		1.75	0
- Berms		2.5	0
- Noise		4	1.25
- Vegetation		1.25	1.25
- Fencing		1.25	0
- Berms		1.5	0
Current Conditions	10%	10	7
Ex. Trail or Sidewalk & Used		10	10
No Trail or Sidewalk, but Used		7	7
No Trail or Sidewalk & Un-Used		5	0
Site Details	5%	5	5
Usable w/out Improvement		5	5
Un-Usable w/ out Improvement		2	0
Public Opinion of APO**	25%	25	25
Support (75%+)		25	25
Oppose (<25%)		0	0
Mix - For vs. Against		10 - 15	10
Jurisdiction	5%	5	5
In City		5	5
In ETJ		3	0
Total	100%	100	87

* Single Family Residential Property ** Adjacent Property Owners





CITY of CEDAR PARK - EVALUATION CRITERIA for HIKE & BIKE CORRIDORS
 Corridor Name: Town Center Trails
 Type: Trail
 Score: A
 Length: 9,885 ft.

Evaluation Criterion	Importance	Total Pts Available	Points
Connectivity	20%	20	12
Schools		6	
Trail-to-Trail		6	6
Parks & Other Amenities		4	4
Major Retail		2	
Major Employers		2	2
Availability	15%	15	15
City Owned		15	15
Entity Owned		10	
Privately Owned			0
- Single Owner		6	
- Common Ownership (HOA)		4	
- Multiple Owners		2	
Proximity to SFR*	20%	20	25
Width of Corridor - Separation		15	15
Elevation - Visibility from Above		-5	
Buffers		10	10
- Visual		6	6
- Vegetation		1.75	1.75
- Fencing		1.75	1.75
- Berms		2.5	2.5
- Noise		4	4
- Vegetation		1.25	1.25
- Fencing		1.25	1.25
- Berms		1.5	1.5
Current Conditions	10%	10	7
Ex. Trail or Sidewalk & Used		10	
No Trail or Sidewalk, but Used		7	7
No Trail or Sidewalk & Un-Used		5	
Site Details	5%	5	2
Usable w/out Improvement		5	
Un-Usable w/ out Improvement		2	2
Public Opinion of APO**	25%	25	25
Support (75%+)		25	25
Oppose (<25%)		0	
Mix - For vs. Against		10 - 15	
Jurisdiction	5%	5	5
In City		5	5
In ETJ		3	
Total	100%	100	91

* Single Family Residential Property ** Adjacent Property Owners

Town Center Trails

Trails are proposed throughout the Town Center development to connect to the Cedar Park Center, Recreation Center, and the future retail that is proposed along 183A. A trail is proposed through the wide median along Discovery Blvd.



Northeast Developer Trails

There are several large undeveloped lots in the far northeast portion of City. If future development were to occur on these properties, then trails should be constructed to provide connectivity to those future homes or future commercial areas.

CITY OF CEDAR PARK - EVALUATION CRITERIA for HIKE & BIKE CORRIDORS			
Corridor Name: Northeast Developer Trails		Score: D	
Type: Developer Trails		Length: 24,110 ft.	
Evaluation Criterion	Importance	Total Pts Available	Points
Connectivity	20%	20	0
Schools		6	
Trail-to-Trail		6	
Parks & Other Amenities		4	
Major Retail		2	
Major Employers		2	
Availability	15%	15	2
City Owned		15	
Entity Owned		10	
Privately Owned		2	2
- Single Owner		6	
- Common Ownership (HOA)		4	
- Multiple Owners		2	2
Proximity to SFR*	20%	20	18
Width of Corridor - Separation		15	15
Elevation - Visibility from Above		-5	
Buffers		10	3
- Visual		6	1.75
- Vegetation		1.75	1.75
- Fencing		1.75	
- Berms		2.5	
- Noise		4	1.25
- Vegetation		1.25	1.25
- Fencing		1.25	
- Berms		1.5	
Current Conditions	10%	10	5
Ex. Trail or Sidewalk & Used		10	
No Trail or Sidewalk, but Used		7	
No Trail or Sidewalk & Un-Used		5	5
Site Details	5%	5	2
Usable w/out Improvement		5	
Un-Usable w/ out Improvement		2	2
Public Opinion of APO**	25%	25	0
Support (75%+)		25	
Oppose (<25%)		0	
Mix - For vs. Against		10 - 15	
Jurisdiction	5%	5	4
In City		5	4
In ETJ		3	
Total	100%	100	31

* Single Family Residential Property ** Adjacent Property Owners





CITY of CEDAR PARK - EVALUATION CRITERIA for HIKE & BIKE CORRIDORS
 Corridor Name: New Hope Dr. (Discovery to Sam Bass Rd) Score: B
 Type: Parkway Sidewalk Length: 19,110 ft.

Evaluation Criterion	Importance	Total Pts Available	Points
Connectivity	20%	20	6
Schools		6	
Trail-to-Trail		6	
Parks & Other Amenities		4	4
Major Retail		2	
Major Employers		2	2
Availability	15%	15	15
City Owned		15	15
Entity Owned		10	
Privately Owned			0
- Single Owner		6	
- Common Ownership (HOA)		4	
- Multiple Owners		2	
Proximity to SFR*	20%	20	18
Width of Corridor - Separation		15	15
Elevation - Visibility from Above		-5	
Buffers		10	3
- Visual		6	1.75
- Vegetation		1.75	1.75
- Fencing		1.75	
- Berms		2.5	
- Noise		4	1.25
- Vegetation		1.25	1.25
- Fencing		1.25	
- Berms		1.5	
Current Conditions	10%	10	5
Ex. Trail or Sidewalk & Used		10	
No Trail or Sidewalk, but Used		7	
No Trail or Sidewalk & Un-Used		5	5
Site Details	5%	5	2
Usable w/out Improvement		5	
Un-Usable w/ out Improvement		2	2
Public Opinion of APO**	25%	25	25
Support (75%+)		25	25
Oppose (<25%)		0	
Mix - For vs. Against		10 - 15	
Jurisdiction	5%	5	5
In City		5	5
In ETJ		3	
Total	100%	100	76

* Single Family Residential Property ** Adjacent Property Owners

New Hope Drive (Discovery Blvd. to Sam Bass Rd.)

As mentioned in Sector 1, New Hope Drive is part of the City's Transportation Master Plan. A proposed ten-foot wide parkway sidewalk should be built alongside the street as New Hope Drive is extended.



Medical Center Area Trails

Trails are proposed along Cottonwood Creek to connect to the Cedar Park Regional Medical Center and other major employers. The hospital has built some trails around a small pond towards the back of their property. Any constructed trails should connect to these.

CITY OF CEDAR PARK - EVALUATION CRITERIA for HIKE & BIKE CORRIDORS			
Corridor Name: Medical Center Area Trails		Score: D	
Type: Trails		Length: 8,750 ft.	
Evaluation Criterion	Importance	Total Pts Available	Points
Connectivity	20%	20	4
Schools		6	
Trail-to-Trail		6	
Parks & Other Amenities		4	
Major Retail		2	2
Major Employers		2	2
Availability	15%	15	2
City Owned		15	
Entity Owned		10	
Privately Owned			2
- Single Owner		6	
- Common Ownership (HOA)		4	
- Multiple Owners		2	2
Proximity to SFR*	20%	20	18
Width of Corridor - Separation		15	15
Elevation - Visibility from Above		-5	
Buffers		10	3
- Visual		6	1.75
- Vegetation		1.75	1.75
- Fencing		1.75	
- Berms		2.5	
- Noise		4	1.25
- Vegetation		1.25	1.25
- Fencing		1.25	
- Berms		1.5	
Current Conditions	10%	10	7
Ex. Trail or Sidewalk & Used		10	
No Trail or Sidewalk, but Used		7	7
No Trail or Sidewalk & Un-Used		5	
Site Details	5%	5	2
Usable w/out Improvement		5	
Un-Usable w/ out Improvement		2	2
Public Opinion of APO**	25%	25	0
Support (75%+)		25	
Oppose (<25%)		0	
Mix - For vs. Against		10 - 15	
Jurisdiction	5%	5	5
In City		5	5
In ETJ		3	
Total	100%	100	38

* Single Family Residential Property ** Adjacent Property Owners





CITY OF CEDAR PARK - EVALUATION CRITERIA for HIKE & BIKE CORRIDORS			
Corridor Name: Cottonwood Creek Sidewalk		Score: C	
Type: Sidewalk		Length: 3,645 ft.	
Evaluation Criterion	Importance	Total Pts Available	Points
Connectivity	20%	20	4
Schools		6	
Trail-to-Trail		6	
Parks & Other Amenities		4	
Major Retail		2	2
Major Employers		2	2
Availability	15%	15	15
City Owned		15	15
Entity Owned		10	
Privately Owned			0
- Single Owner		6	
- Common Ownership (HOA)		4	
- Multiple Owners		2	
Proximity to SFR*	20%	20	18
Width of Corridor - Separation		15	15
Elevation - Visibility from Above		-5	
Buffers		10	3
- Visual		6	1.75
- Vegetation		1.75	1.75
- Fencing		1.75	
- Berms		2.5	
- Noise		4	1.25
- Vegetation		1.25	1.25
- Fencing		1.25	
- Berms		1.5	
Current Conditions	10%	10	7
Ex. Trail or Sidewalk & Used		10	
No Trail or Sidewalk, but Used		7	7
No Trail or Sidewalk & Un-Used		5	
Site Details	5%	5	5
Usable w/out Improvement		5	5
Un-Usable w/ out Improvement		2	
Public Opinion of APO**	25%	25	0
Support (75%+)		25	
Oppose (<25%)		0	
Mix - For vs. Against		10 - 15	
Jurisdiction	5%	5	5
In City		5	5
In ETJ		3	
Total	100%	100	54

* Single Family Residential Property ** Adjacent Property Owners

Cottonwood Creek Sidewalk

A sidewalk is proposed along Cottonwood Creek Trail. This sidewalk will create a connection to the Cottonwood Creek trails around the Cedar Park Regional Medical Center, as well as to Whitestone Blvd. There are several major employers off of Cottonwood Creek Trail, so this sidewalk will provide them access to the Medical Center and the 1890 Ranch retail shopping area.



Whitestone Blvd. (Discovery Blvd. to Vista Ridge Pkwy.)

Although Whitestone Blvd. is a TxDOT road, a ten to fifteen foot multi-use parkway sidewalk should be constructed on one side of the street. Whitestone Blvd. serves as a major corridor by connecting the entire City from east to west. Providing a safe, off-street facility for both pedestrians and bicyclists should be a priority. This connection will give access to several destinations and serve as a major spine corridor in the trail system.

CITY OF CEDAR PARK - EVALUATION CRITERIA for HIKE & BIKE CORRIDORS			
Corridor Name: Whitestone Blvd (Discovery to Vista Ridge)		Score: B	
Type: Parkway Sidewalk		Length: 16,955 ft.	
Evaluation Criterion	Importance	Total Pts Available	Points
Connectivity	20%	20	4
Schools		6	
Trail-to-Trail		6	
Parks & Other Amenities		4	
Major Retail		2	2
Major Employers		2	2
Availability	15%	15	10
City Owned		15	
Entity Owned		10	10
Privately Owned			0
- Single Owner		6	
- Common Ownership (HOA)		4	
- Multiple Owners		2	
Proximity to SFR*	20%	20	18
Width of Corridor - Separation		15	15
Elevation - Visibility from Above		-5	
Buffers		10	3
- Visual		6	1.75
- Vegetation		1.75	1.75
- Fencing		1.75	
- Berms		2.5	
- Noise		4	1.25
- Vegetation		1.25	1.25
- Fencing		1.25	
- Berms		1.5	
Current Conditions	10%	10	7
Ex. Trail or Sidewalk & Used		10	
No Trail or Sidewalk, but Used		7	7
No Trail or Sidewalk & Un-Used		5	
Site Details	5%	5	5
Usable w/out Improvement		5	5
Un-Usable w/ out Improvement		2	
Public Opinion of APO**	25%	25	25
Support (75%+)		25	25
Oppose (<25%)		0	
Mix - For vs. Against		10 - 15	
Jurisdiction	5%	5	5
In City		5	5
In ETJ		3	
Total	100%	100	74

* Single Family Residential Property ** Adjacent Property Owners





CITY of CEDAR PARK - EVALUATION CRITERIA for HIKE & BIKE CORRIDORS
 Corridor Name: Eastern Developer Trails
 Type: Developer Trails
 Length: 10,535 ft.
 Score: D

Evaluation Criterion	Importance	Total Pts Available	Points
Connectivity	20%	20	6
Schools		6	
Trail-to-Trail		6	
Parks & Other Amenities		4	4
Major Retail		2	
Major Employers		2	2
Availability	15%	15	2
City Owned		15	
Entity Owned		10	
Privately Owned			2
- Single Owner		6	
- Common Ownership (HOA)		4	
- Multiple Owners		2	2
Proximity to SFR*	20%	20	18
Width of Corridor - Separation		15	15
Elevation - Visibility from Above		-5	
Buffers		10	3
- Visual		6	1.75
- Vegetation		1.75	1.75
- Fencing		1.75	
- Berms		2.5	
- Noise		4	1.25
- Vegetation		1.25	1.25
- Fencing		1.25	
- Berms		1.5	
Current Conditions	10%	10	5
Ex. Trail or Sidewalk & Used		10	
No Trail or Sidewalk, but Used		7	
No Trail or Sidewalk & Un-Used		5	5
Site Details	5%	5	2
Usable w/out Improvement		5	
Un-Usable w/ out Improvement		2	2
Public Opinion of APO**	25%	25	0
Support (75%+)		25	
Oppose (<25%)		0	
Mix - For vs. Against		10 - 15	
Jurisdiction	5%	5	5
In City		5	5
In ETJ		3	
Total	100%	100	38

* Single Family Residential Property ** Adjacent Property Owners

Eastern Developer Trails

Similar to properties in the northeast there are some currently undeveloped large properties in the far eastern portion of the City. If these properties were ever sold for future development, such as residential or commercial use, then trails should be an important part of the infrastructure to connect to other areas of the community. One major destination in this area is the proposed water park. Trails that connect to this future destination are a high priority.



Silverado Springs Park North

Trails are proposed along the north fork of Brushy Creek and Silverado Springs Park North. This corridor will provide connections to the existing Brushy Creek Regional Trail and to the proposed water park.

CITY OF CEDAR PARK - EVALUATION CRITERIA for HIKE & BIKE CORRIDORS			
Corridor Name: Silverado Springs Park North		Score: B	
Type: Trail		Length: 9,270 ft.	
Evaluation Criterion	Importance	Total Pts Available	Points
Connectivity	20%	20	12
Schools		6	6
Trail-to-Trail		6	6
Parks & Other Amenities		4	4
Major Retail		2	2
Major Employers		2	2
Availability	15%	15	0
City Owned		15	0
Entity Owned		10	0
Privately Owned		0	0
- Single Owner		6	0
- Common Ownership (HOA)		4	0
- Multiple Owners		2	0
Proximity to SFR*	20%	20	21
Width of Corridor - Separation		15	15
Elevation - Visibility from Above		-5	0
Buffers		10	6
- Visual		6	3.5
- Vegetation		1.75	1.75
- Fencing		1.75	1.75
- Berms		2.5	2.5
- Noise		4	2.5
- Vegetation		1.25	1.25
- Fencing		1.25	1.25
- Berms		1.5	1.5
Current Conditions	10%	10	5
Ex. Trail or Sidewalk & Used		10	5
No Trail or Sidewalk, but Used		7	5
No Trail or Sidewalk & Un-Used		5	5
Site Details	5%	5	2
Usable w/out Improvement		5	2
Un-Usable w/ out Improvement		2	2
Public Opinion of APO**	25%	25	25
Support (75%+)		25	25
Oppose (<25%)		0	0
Mix - For vs. Against		10 - 15	0
Jurisdiction	5%	5	5
In City		5	5
In ETJ		3	0
Total	100%	100	70

* Single Family Residential Property ** Adjacent Property Owners





CITY of CEDAR PARK - EVALUATION CRITERIA for HIKE & BIKE CORRIDORS
 Corridor Name: Silverado Springs Drainage Corridor
 Type: Trail
 Score: A
 Length: 4,875 ft.

Evaluation Criterion	Importance	Total Pts Available	Points
Connectivity	20%	20	18
Schools		6	6
Trail-to-Trail		6	6
Parks & Other Amenities		4	4
Major Retail		2	2
Major Employers		2	
Availability	15%	15	6
City Owned		15	
Entity Owned		10	
Privately Owned			6
- Single Owner		6	6
- Common Ownership (HOA)		4	
- Multiple Owners		2	
Proximity to SFR*	20%	20	21
Width of Corridor - Separation		15	15
Elevation - Visibility from Above		-5	
Buffers		10	6
- Visual		6	3.5
- Vegetation		1.75	1.75
- Fencing		1.75	1.75
- Berms		2.5	
- Noise		4	2.5
- Vegetation		1.25	1.25
- Fencing		1.25	1.25
- Berms		1.5	
Current Conditions	10%	10	5
Ex. Trail or Sidewalk & Used		10	
No Trail or Sidewalk, but Used		7	
No Trail or Sidewalk & Un-Used		5	5
Site Details	5%	5	2
Usable w/out Improvement		5	
Un-Usable w/ out Improvement		2	2
Public Opinion of APO**	25%	25	25
Support (75%+)		25	25
Oppose (<25%)		0	
Mix - For vs. Against		10 - 15	
Jurisdiction	5%	5	4
In City		5	4
In ETJ		3	
Total	100%	100	81

* Single Family Residential Property ** Adjacent Property Owners

Silverado Springs Drainage Corridor

This trail corridor was proposed by residents in the public input process. It provides a safe, off-street connection from the apartment complexes and the surrounding neighborhood to the Leander ISD school properties.



Whitestone Blvd. (Parmer Lane to Sam Bass Rd.)

The parkway sidewalk along Whitestone Blvd. should continue to the eastern limits of the City. This will provide a long term connection into Round Rock and other destinations such as Williamson County Regional Park off Sam Bass Rd.

CITY OF CEDAR PARK - EVALUATION CRITERIA for HIKE & BIKE CORRIDORS			
Corridor Name: Whitestone Blvd. (Parmer to Sam Bass Rd)		Score: B	
Type: Parkway Sidewalk		Length: 7,480 ft.	
Evaluation Criterion	Importance	Total Pts Available	Points
Connectivity	20%	20	6
Schools		6	
Trail-to-Trail		6	
Parks & Other Amenities		4	4
Major Retail		2	
Major Employers		2	2
Availability	15%	15	15
City Owned		15	15
Entity Owned		10	
Privately Owned			0
- Single Owner		6	
- Common Ownership (HOA)		4	
- Multiple Owners		2	
Proximity to SFR*	20%	20	15
Width of Corridor - Separation		15	15
Elevation - Visibility from Above		-5	
Buffers		10	0
- Visual		6	0
- Vegetation		1.75	
- Fencing		1.75	
- Berms		2.5	
- Noise		4	0
- Vegetation		1.25	
- Fencing		1.25	
- Berms		1.5	
Current Conditions	10%	10	7
Ex. Trail or Sidewalk & Used		10	
No Trail or Sidewalk, but Used		7	7
No Trail or Sidewalk & Un-Used		5	
Site Details	5%	5	2
Usable w/out Improvement		5	
Un-Usable w/ out Improvement		2	2
Public Opinion of APO**	25%	25	25
Support (75%+)		25	25
Oppose (<25%)		0	
Mix - For vs. Against		10 - 15	
Jurisdiction	5%	5	5
In City		5	5
In ETJ		3	
Total	100%	100	75

* Single Family Residential Property ** Adjacent Property Owners





CITY of CEDAR PARK - EVALUATION CRITERIA for HIKE & BIKE CORRIDORS
 Corridor Name: Parmer Lane (sector limit to city limits) Score: B
 Type: Parkway Sidewalk Length: 15,360 ft.

Evaluation Criterion	Importance	Total Pts Available	Points
Connectivity	20%	20	10
Schools		6	
Trail-to-Trail		6	6
Parks & Other Amenities		4	
Major Retail		2	2
Major Employers		2	2
Availability	15%	15	10
City Owned		15	
Entity Owned		10	10
Privately Owned			0
- Single Owner		6	
- Common Ownership (HOA)		4	
- Multiple Owners		2	
Proximity to SFR*	20%	20	15
Width of Corridor - Separation		15	15
Elevation - Visibility from Above		-5	
Buffers		10	0
- Visual		6	0
- Vegetation		1.75	
- Fencing		1.75	
- Berms		2.5	
- Noise		4	0
- Vegetation		1.25	
- Fencing		1.25	
- Berms		1.5	
Current Conditions	10%	10	7
Ex. Trail or Sidewalk & Used		10	
No Trail or Sidewalk, but Used		7	7
No Trail or Sidewalk & Un-Used		5	
Site Details	5%	5	5
Usable w/out Improvement		5	5
Un-Usable w/ out Improvement		2	
Public Opinion of APO**	25%	25	25
Support (75%+)		25	25
Oppose (<25%)		0	
Mix - For vs. Against		10 - 15	
Jurisdiction	5%	5	5
In City		5	5
In ETJ		3	
Total	100%	100	77

* Single Family Residential Property ** Adjacent Property Owners

Parmer Lane

This Master Plan proposes that a parkway sidewalk be built along at least one side of Parmer Lane north of Whitestone Blvd., and along both sides south of Whitestone Blvd.

This is a major arterial road which connects the eastern portion of the City to several destinations and other surrounding communities. Parmer Lane also connects to the existing Brushy Creek Regional Trail.



Central Developer Trails

Developer trails are proposed through the central portion of this sector, if future development were to occur. These future developer trails will connect the residential areas to destinations such as 1890 Ranch shopping area and the Cedar Park Medical Center.

CITY OF CEDAR PARK - EVALUATION CRITERIA for HIKE & BIKE CORRIDORS			
Corridor Name: Central Developer Trails		Score: D	
Type: Developer Trails		Length: 16,700 ft.	
Evaluation Criterion	Importance	Total Pts Available	Points
Connectivity	20%	20	4
Schools		6	
Trail-to-Trail		6	
Parks & Other Amenities		4	
Major Retail		2	2
Major Employers		2	2
Availability	15%	15	2
City Owned		15	
Entity Owned		10	
Privately Owned			2
- Single Owner		6	
- Common Ownership (HOA)		4	
- Multiple Owners		2	2
Proximity to SFR*	20%	20	13
Width of Corridor - Separation		15	15
Elevation - Visibility from Above		-5	-5
Buffers		10	3
- Visual		6	1.75
- Vegetation		1.75	1.75
- Fencing		1.75	
- Berms		2.5	
- Noise		4	1.25
- Vegetation		1.25	1.25
- Fencing		1.25	
- Berms		1.5	
Current Conditions	10%	10	5
Ex. Trail or Sidewalk & Used		10	
No Trail or Sidewalk, but Used		7	
No Trail or Sidewalk & Un-Used		5	5
Site Details	5%	5	2
Usable w/out Improvement		5	
Un-Usable w/ out Improvement		2	2
Public Opinion of APO**	25%	25	0
Support (75%+)		25	
Oppose (<25%)		0	
Mix - For vs. Against		10 - 15	
Jurisdiction	5%	5	5
In City		5	5
In ETJ		3	
Total	100%	100	31

* Single Family Residential Property ** Adjacent Property Owners





CITY OF CEDAR PARK - EVALUATION CRITERIA for HIKE & BIKE CORRIDORS			
Corridor Name: Gas Line Easement Trail			
Type: Trail			
Score: A			
Length: 6,075 ft.			
Evaluation Criterion	Importance	Total Pts Available	Points
Connectivity	20%	20	6
Schools		6	6
Trail-to-Trail		6	
Parks & Other Amenities		4	
Major Retail		2	
Major Employers		2	
Availability	15%	15	10
City Owned		15	
Entity Owned		10	10
Privately Owned			0
- Single Owner		6	
- Common Ownership (HOA)		4	
- Multiple Owners		2	
Proximity to SFR*	20%	20	21
Width of Corridor - Separation		15	15
Elevation - Visibility from Above		-5	
Buffers		10	6
- Visual		6	3.5
- Vegetation		1.75	1.75
- Fencing		1.75	1.75
- Berms		2.5	
- Noise		4	2.5
- Vegetation		1.25	1.25
- Fencing		1.25	1.25
- Berms		1.5	
Current Conditions	10%	10	10
Ex. Trail or Sidewalk & Used		10	10
No Trail or Sidewalk, but Used		7	
No Trail or Sidewalk & Un-Used		5	
Site Details	5%	5	5
Usable w/out Improvement		5	5
Un-Usable w/ out Improvement		2	
Public Opinion of APO**	25%	25	25
Support (75%+)		25	25
Oppose (<25%)		0	
Mix - For vs. Against		10 - 15	
Jurisdiction	5%	5	4
In City		5	4
In ETJ		3	
Total	100%	100	81

* Single Family Residential Property ** Adjacent Property Owners

Gas Line Easement Trail

The Lone Star Gas Line Easement passes through much of central Cedar Park. This section of the easement trail will connect from 183A to Creek Vista Blvd. This trail will provide a safe, off-street connection from the surrounding neighborhoods to the Leander ISD school properties.



183A (Park St. north to City Limits)

A multi-use hike and bike trail is proposed to follow along the 183A toll road. This trail will provide a safe, off-street facility for commuting purposes. Bicyclists and pedestrians can use the trail to travel through Cedar Park and connect to the many destinations along 183A.

The Central Texas Regional Mobility Authority currently has designs for a trail along 183A from New Hope Drive north to the City Limits, and from Whitestone Blvd. south to Brushy Creek.

CITY of CEDAR PARK - EVALUATION CRITERIA for HIKE & BIKE CORRIDORS			
Corridor Name: 183A (Park St. to city limits)		Score: A	
Type: Trail		Length: 36,580 ft.	
Evaluation Criterion	Importance	Total Pts Available	Points
Connectivity	20%	20	14
Schools		6	6
Trail-to-Trail		6	6
Parks & Other Amenities		4	4
Major Retail		2	2
Major Employers		2	2
Availability	15%	15	10
City Owned		15	10
Entity Owned		10	10
Privately Owned			0
- Single Owner		6	6
- Common Ownership (HOA)		4	4
- Multiple Owners		2	2
Proximity to SFR*	20%	20	25
Width of Corridor - Separation		15	15
Elevation - Visibility from Above		-5	0
Buffers		10	10
- Visual		6	6
- Vegetation		1.75	1.75
- Fencing		1.75	1.75
- Berms		2.5	2.5
- Noise		4	4
- Vegetation		1.25	1.25
- Fencing		1.25	1.25
- Berms		1.5	1.5
Current Conditions	10%	10	5
Ex. Trail or Sidewalk & Used		10	10
No Trail or Sidewalk, but Used		7	7
No Trail or Sidewalk & Un-Used		5	5
Site Details	5%	5	2
Usable w/out Improvement		5	5
Un-Usable w/ out Improvement		2	2
Public Opinion of APO**	25%	25	25
Support (75%+)		25	25
Oppose (<25%)		0	0
Mix - For vs. Against		10 - 15	10 - 15
Jurisdiction	5%	5	5
In City		5	5
In ETJ		3	3
Total	100%	100	86

* Single Family Residential Property ** Adjacent Property Owners



Sector 4 Trail Opportunities

The existing Brushy Creek Regional Trail is located along the southern boundary of this sector. This is a major destination that the proposed trails should connect to. Residents in the Forest Oaks and Silverado Springs neighborhoods expressed interest and desire to connect their neighborhoods to the Brushy Creek Regional Trail.

Other major destinations in this sector include the Leander ISD properties of Vista Ridge High School, Artie Henry Middle School, the newly opened Ronald Reagan Elementary School, and the future LISD football stadium.

The eastern portion of this sector is largely undeveloped. Similar to Sector 3, as these areas develop, the developers should contribute to the citywide trails network by constructing segments of the proposed trails.





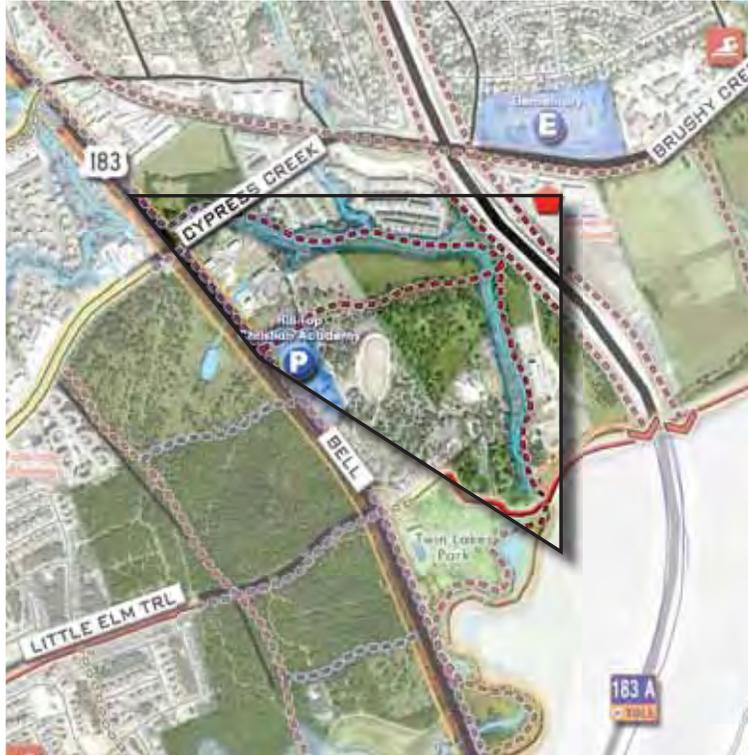
Twin Lakes Park Trail

This Master Plan proposes that the existing Brushy Creek Regional Trail be extended through Twin Lakes Park, around the lakes. This extension has the possibility of connecting to the trails around Lake Cedar Park and the Lakeline Village PUD Park. Crossing the trail along the creek, under Bell St. may be difficult because it may be too shallow for an adequate underpass. If that is the case, then a safe pedestrian crossing over Bell St. is needed with traffic lights, cross walks, and pedestrian signals.

CITY OF CEDAR PARK - EVALUATION CRITERIA for HIKE & BIKE CORRIDORS			
Corridor Name: Twin Lakes Park Trail		Score: A	
Type: Trail		Length: 3,895 ft.	
Evaluation Criterion	Importance	Total Pts Available	Points
Connectivity	20%	20	12
Schools		6	6
Trail-to-Trail		6	6
Parks & Other Amenities		4	4
Major Retail		2	2
Major Employers		2	2
Availability	15%	15	15
City Owned		15	15
Entity Owned		10	0
Privately Owned			0
- Single Owner		6	
- Common Ownership (HOA)		4	
- Multiple Owners		2	
Proximity to SFR*	20%	20	22
Width of Corridor - Separation		15	15
Elevation - Visibility from Above		-5	
Buffers		10	7
- Visual		6	4.25
- Vegetation		1.75	1.75
- Fencing		1.75	
- Berms		2.5	2.5
- Noise		4	2.75
- Vegetation		1.25	1.25
- Fencing		1.25	
- Berms		1.5	1.5
Current Conditions	10%	10	7
Ex. Trail or Sidewalk & Used		10	
No Trail or Sidewalk, but Used		7	7
No Trail or Sidewalk & Un-Used		5	
Site Details	5%	5	5
Usable w/out Improvement		5	5
Un-Usable w/ out Improvement		2	
Public Opinion of APO**	25%	25	25
Support (75%+)		25	25
Oppose (<25%)		0	
Mix - For vs. Against		10 - 15	
Jurisdiction	5%	5	5
In City		5	5
In ETJ		3	
Total	100%	100	91

* Single Family Residential Property ** Adjacent Property Owners





CITY of CEDAR PARK - EVALUATION CRITERIA for HIKE & BIKE CORRIDORS
 Corridor Name: Cluck Creek Trail
 Type: Trail
 Score: B
 Length: 6,930 ft.

Evaluation Criterion	Importance	Total Pts Available	Points
Connectivity	20%	20	18
Schools		6	6
Trail-to-Trail		6	6
Parks & Other Amenities		4	4
Major Retail		2	
Major Employers		2	2
Availability	15%	15	2
City Owned		15	
Entity Owned		10	
Privately Owned			2
- Single Owner		6	
- Common Ownership (HOA)		4	
- Multiple Owners		2	2
Proximity to SFR*	20%	20	18
Width of Corridor - Separation		15	15
Elevation - Visibility from Above		-5	
Buffers		10	3
- Visual		6	1.75
- Vegetation		1.75	1.75
- Fencing		1.75	
- Berms		2.5	
- Noise		4	1.25
- Vegetation		1.25	1.25
- Fencing		1.25	
- Berms		1.5	
Current Conditions	10%	10	5
Ex. Trail or Sidewalk & Used		10	
No Trail or Sidewalk, but Used		7	
No Trail or Sidewalk & Un-Used		5	5
Site Details	5%	5	2
Usable w/out Improvement		5	
Un-Usable w/ out Improvement		2	2
Public Opinion of APO**	25%	25	25
Support (75%+)		25	25
Oppose (<25%)		0	
Mix - For vs. Against		10 - 15	
Jurisdiction	5%	5	5
In City		5	5
In ETJ		3	
Total	100%	100	75

* Single Family Residential Property ** Adjacent Property Owners

Cluck Creek Trail

Cluck Creek extends through the southwest portion of this sector, and empties into Brushy Creek. This provides a connection from the existing Brushy Creek Regional Trail to Cypress Creek Rd. These proposed trails also connect Hill Top Christian Academy to the existing regional trail network.



Bell Blvd. (Park St. to Lakeline Blvd.)

As mentioned previously, Bell Street is part of the City's Transportation Master Plan and is identified to have a ten-foot wide meandering sidewalk added when it is improved. This Master Plan again recommends the sidewalk be a parkway sidewalk. This parkway sidewalk will serve as a key spine corridor and connect to the existing Brushy Creek Regional Trail. TxDOT is to install sidewalks for a large section of Bell Blvd.

CITY OF CEDAR PARK - EVALUATION CRITERIA for HIKE & BIKE CORRIDORS			
Corridor Name: Bell Street (Park St. to Lakeline Blvd.)		Score: A	
Type: Parkway Sidewalk		Length: 12,645 ft.	
Evaluation Criterion	Importance	Total Pts Available	Points
Connectivity	20%	20	20
Schools		6	6
Trail-to-Trail		6	6
Parks & Other Amenities		4	4
Major Retail		2	2
Major Employers		2	2
Availability	15%	15	15
City Owned		15	15
Entity Owned		10	
Privately Owned			0
- Single Owner		6	
- Common Ownership (HOA)		4	
- Multiple Owners		2	
Proximity to SFR*	20%	20	15
Width of Corridor - Separation		15	15
Elevation - Visibility from Above		-5	
Buffers		10	0
- Visual		6	0
- Vegetation		1.75	
- Fencing		1.75	
- Berms		2.5	
- Noise		4	0
- Vegetation		1.25	
- Fencing		1.25	
- Berms		1.5	
Current Conditions	10%	10	7
Ex. Trail or Sidewalk & Used		10	
No Trail or Sidewalk, but Used		7	7
No Trail or Sidewalk & Un-Used		5	
Site Details	5%	5	2
Usable w/out Improvement		5	
Un-Usable w/ out Improvement		2	2
Public Opinion of APO**	25%	25	25
Support (75%+)		25	25
Oppose (<25%)		0	
Mix - For vs. Against		10 - 15	
Jurisdiction	5%	5	5
In City		5	5
In ETJ		3	
Total	100%	100	89

* Single Family Residential Property ** Adjacent Property Owners





CITY of CEDAR PARK - EVALUATION CRITERIA for HIKE & BIKE CORRIDORS
 Corridor Name: Cap Metro Rail Trail
 Type: Trail
 Length: 14,700 ft.
 Score: B

Evaluation Criterion	Importance	Total Pts Available	Points
Connectivity	20%	20	18
Schools		6	6
Trail-to-Trail		6	6
Parks & Other Amenities		4	4
Major Retail		2	
Major Employers		2	2
Availability	15%	15	10
City Owned		15	
Entity Owned		10	10
Privately Owned			0
- Single Owner		6	
- Common Ownership (HOA)		4	
- Multiple Owners		2	
Proximity to SFR*	20%	20	8
Width of Corridor - Separation		15	
Elevation - Visibility from Above		-5	
Buffers		10	8
- Visual		6	4.25
- Vegetation		1.75	1.75
- Fencing		1.75	
- Berms		2.5	2.5
- Noise		4	3.75
- Vegetation		1.25	1.25
- Fencing		1.25	
- Berms		1.5	2.5
Current Conditions	10%	10	5
Ex. Trail or Sidewalk & Used		10	
No Trail or Sidewalk, but Used		7	
No Trail or Sidewalk & Un-Used		5	5
Site Details	5%	5	5
Usable w/out Improvement		5	5
Un-Usable w/ out Improvement		2	
Public Opinion of APO**	25%	25	25
Support (75%+)		25	25
Oppose (<25%)		0	
Mix - For vs. Against		10 - 15	
Jurisdiction	5%	5	5
In City		5	5
In ETJ		3	
Total	100%	100	76

* Single Family Residential Property ** Adjacent Property Owners

Cap Metro Rail Trail

As mentioned earlier in this Chapter, the Cap Metro Rail corridor is likely too narrow to accommodate a trail alongside the tracks. However, because of the great potential the corridor has, a trail is proposed if the rail is ever abandoned at any point in the future.



183A Trail (Park St. south to Brushy Creek)

The 183A hike and bike trail is proposed to connect to the existing Brushy Creek Regional Trail. This hike and bike trail will then travel through the entire City of Cedar Park, connecting to several destinations. The first phase of this trail is expected to be construction in 2010. The trail is funded and will be constructed by the Central Texas Regional Mobility Authority.

CITY OF CEDAR PARK - EVALUATION CRITERIA for HIKE & BIKE CORRIDORS			
Corridor Name: 183A Trail (Park St. to Brushy Creek)		Score: A	
Type: Trail		Length: 14,305 ft.	
Evaluation Criterion	Importance	Total Pts Available	Points
Connectivity	20%	20	14
Schools		6	6
Trail-to-Trail		6	6
Parks & Other Amenities		4	
Major Retail		2	
Major Employers		2	2
Availability	15%	15	10
City Owned		15	
Entity Owned		10	10
Privately Owned			0
- Single Owner		6	
- Common Ownership (HOA)		4	
- Multiple Owners		2	
Proximity to SFR*	20%	20	20
Width of Corridor - Separation		15	15
Elevation - Visibility from Above		-5	
Buffers		10	5
- Visual		6	2.5
- Vegetation		1.75	
- Fencing		1.75	
- Berms		2.5	2.5
- Noise		4	2.5
- Vegetation		1.25	
- Fencing		1.25	
- Berms		1.5	2.5
Current Conditions	10%	10	5
Ex. Trail or Sidewalk & Used		10	
No Trail or Sidewalk, but Used		7	
No Trail or Sidewalk & Un-Used		5	5
Site Details	5%	5	2
Usable w/out Improvement		5	
Un-Usable w/ out Improvement		2	2
Public Opinion of APO**	25%	25	25
Support (75%+)		25	25
Oppose (<25%)		0	
Mix - For vs. Against		10 - 15	
Jurisdiction	5%	5	5
In City		5	5
In ETJ		3	
Total	100%	100	81

* Single Family Residential Property ** Adjacent Property Owners





CITY of CEDAR PARK - EVALUATION CRITERIA for HIKE & BIKE CORRIDORS
 Corridor Name: Forest Oaks Park Greenbelt
 Type: Trail
 Score: A
 Length: 5,615 ft.

Evaluation Criterion	Importance	Total Pts Available	Points
Connectivity	20%	20	10
Schools		6	6
Trail-to-Trail		6	6
Parks & Other Amenities		4	4
Major Retail		2	2
Major Employers		2	2
Availability	15%	15	4
City Owned		15	15
Entity Owned		10	10
Privately Owned		10	4
- Single Owner		6	6
- Common Ownership (HOA)		4	4
- Multiple Owners		2	2
Proximity to SFR*	20%	20	25
Width of Corridor - Separation		15	15
Elevation - Visibility from Above		-5	0
Buffers		10	10
- Visual		6	6
- Vegetation		1.75	1.75
- Fencing		1.75	1.75
- Berms		2.5	2.5
- Noise		4	4
- Vegetation		1.25	1.25
- Fencing		1.25	1.25
- Berms		1.5	1.5
Current Conditions	10%	10	10
Ex. Trail or Sidewalk & Used		10	10
No Trail or Sidewalk, but Used		7	7
No Trail or Sidewalk & Un-Used		5	5
Site Details	5%	5	2
Usable w/out Improvement		5	5
Un-Usable w/ out Improvement		2	2
Public Opinion of APO**	25%	25	25
Support (75%+)		25	25
Oppose (<25%)		0	0
Mix - For vs. Against		10 - 15	10 - 15
Jurisdiction	5%	5	5
In City		5	5
In ETJ		3	3
Total	100%	100	81

* Single Family Residential Property ** Adjacent Property Owners

Forest Oaks Park Greenbelt

This section of trails received the highest amount of support during the public input process. The residents in these neighborhoods wanted the trails to continue throughout the entire greenbelt, and most importantly they want to connect their neighborhood trail system to the Brushy Creek Regional Trail just to the south. Because of this, the development of these trails and providing a connection over Brushy Creek Rd. to the Brushy Creek Regional Trail is a high priority.



Forest Oaks to Brushy Creek Trails

Trails are proposed in the south portion of the City. These trails are significantly important because they will provide connections from the neighborhoods north of Brushy Creek Rd. to the Brushy Creek Regional Trail. All residents who attended the public meetings were supportive of developing trails somewhere in this area or along BMC Dr. to create this vital connection. The City should actively seek acquisition or easements to build a trail connection. Once the initial connection is built, any future development should construct trails to connect to the overall system.

CITY OF CEDAR PARK - EVALUATION CRITERIA for HIKE & BIKE CORRIDORS			
Corridor Name: Forest Oak to Brushy Creek Trails		Score: B	
Type: Trail		Length: 5,710 ft.	
Evaluation Criterion	Importance	Total Pts Available	Points
Connectivity	20%	20	10
Schools		6	6
Trail-to-Trail		6	6
Parks & Other Amenities		4	4
Major Retail		2	2
Major Employers		2	2
Availability	15%	15	6
City Owned		15	6
Entity Owned		10	6
Privately Owned			6
- Single Owner		6	6
- Common Ownership (HOA)		4	4
- Multiple Owners		2	2
Proximity to SFR*	20%	20	18
Width of Corridor - Separation		15	15
Elevation - Visibility from Above		-5	0
Buffers		10	3
- Visual		6	1.75
- Vegetation		1.75	1.75
- Fencing		1.75	1.75
- Berms		2.5	2.5
- Noise		4	1.25
- Vegetation		1.25	1.25
- Fencing		1.25	1.25
- Berms		1.5	1.5
Current Conditions	10%	10	5
Ex. Trail or Sidewalk & Used		10	10
No Trail or Sidewalk, but Used		7	7
No Trail or Sidewalk & Un-Used		5	5
Site Details	5%	5	2
Usable w/out Improvement		5	5
Un-Usable w/ out Improvement		2	2
Public Opinion of APO**	25%	25	25
Support (75%+)		25	25
Oppose (<25%)		0	0
Mix - For vs. Against		10 - 15	10 - 15
Jurisdiction	5%	5	5
In City		5	5
In ETJ		3	3
Total	100%	100	71

* Single Family Residential Property ** Adjacent Property Owners





CITY OF CEDAR PARK - EVALUATION CRITERIA for HIKE & BIKE CORRIDORS
 Corridor Name: Brushy Creek Road (east of Parmer Lane) Score: A
 Type: Parkway Sidewalk Length: 6,105 ft.

Evaluation Criterion	Importance	Total Pts Available	Points
Connectivity	20%	20	10
Schools		6	6
Trail-to-Trail		6	6
Parks & Other Amenities		4	4
Major Retail		2	2
Major Employers		2	2
Availability	15%	15	15
City Owned		15	15
Entity Owned		10	10
Privately Owned			0
- Single Owner		6	6
- Common Ownership (HOA)		4	4
- Multiple Owners		2	2
Proximity to SFR*	20%	20	22
Width of Corridor - Separation		15	15
Elevation - Visibility from Above		-5	7
Buffers		10	7
- Visual		6	4.25
- Vegetation		1.75	1.75
- Fencing		1.75	1.75
- Berms		2.5	2.5
- Noise		4	2.75
- Vegetation		1.25	1.25
- Fencing		1.25	1.25
- Berms		1.5	1.5
Current Conditions	10%	10	5
Ex. Trail or Sidewalk & Used		10	10
No Trail or Sidewalk, but Used		7	7
No Trail or Sidewalk & Un-Used		5	5
Site Details	5%	5	2
Usable w/out Improvement		5	5
Un-Usable w/ out Improvement		2	2
Public Opinion of APO**	25%	25	25
Support (75%+)		25	25
Oppose (<25%)		0	0
Mix - For vs. Against		10 - 15	10 - 15
Jurisdiction	5%	5	5
In City		5	5
In ETJ		3	3
Total	100%	100	84

* Single Family Residential Property ** Adjacent Property Owners

Brushy Creek Road (east of Parmer Lane)

Brushy Creek Road in this section of the City is mostly a rural two lane road. A parkway sidewalk should be added if the road were ever expanded or improved. Because this road serves a major corridor to Brushy Creek Lake Park and Champion Park, as well as into Round Rock, it is unlikely that it will remain a two lane rural road.



Brushy Creek Road (west of Parmer Lane)

Again, this serves as a major corridor to the existing Brushy Creek Regional Trail. This segment of Brushy Creek Road is a wider four lane road. It connects to the Vista Ridge Pkwy. trail and the Leander ISD school properties. A parkway sidewalk should be built on at least one side of the street. This Master Plan recommends the parkway sidewalk be placed on the north side since it will provide greater connectivity to the schools and the surrounding neighborhoods. A safe pedestrian crossing will then be needed at Parmer Lane to allow access to the Brushy Creek Regional Trail and the Brushy Creek Sports Park.

CITY OF CEDAR PARK - EVALUATION CRITERIA for HIKE & BIKE CORRIDORS			
Corridor Name: Brushy Creek Road (west of Parmer Lane)		Score: B	
Type: Parkway Sidewalk		Length: 11,715 ft.	
Evaluation Criterion	Importance	Total Pts Available	Points
Connectivity	20%	20	4
Schools		6	
Trail-to-Trail		6	
Parks & Other Amenities		4	4
Major Retail		2	
Major Employers		2	
Availability	15%	15	15
City Owned		15	15
Entity Owned		10	
Privately Owned			0
- Single Owner		6	
- Common Ownership (HOA)		4	
- Multiple Owners		2	
Proximity to SFR*	20%	20	18
Width of Corridor - Separation		15	15
Elevation - Visibility from Above		-5	
Buffers		10	3
- Visual		6	1.75
- Vegetation		1.75	1.75
- Fencing		1.75	
- Berms		2.5	
- Noise		4	1.25
- Vegetation		1.25	1.25
- Fencing		1.25	
- Berms		1.5	
Current Conditions	10%	10	5
Ex. Trail or Sidewalk & Used		10	
No Trail or Sidewalk, but Used		7	
No Trail or Sidewalk & Un-Used		5	5
Site Details	5%	5	2
Usable w/out Improvement		5	
Un-Usable w/ out Improvement		2	2
Public Opinion of APO**	25%	25	25
Support (75%+)		25	25
Oppose (<25%)		0	
Mix - For vs. Against		10 - 15	
Jurisdiction	5%	5	5
In City		5	5
In ETJ		3	
Total	100%	100	74

* Single Family Residential Property ** Adjacent Property Owners





CITY OF CEDAR PARK - EVALUATION CRITERIA for HIKE & BIKE CORRIDORS
 Corridor Name: Brushy Creek Recreation Park Trails
 Type: Trail
 Score: B
 Length: 4,370 ft.

Evaluation Criterion	Importance	Total Pts Available	Points
Connectivity	20%	20	10
Schools		6	6
Trail-to-Trail		6	6
Parks & Other Amenities		4	4
Major Retail		2	2
Major Employers		2	2
Availability	15%	15	15
City Owned		15	15
Entity Owned		10	10
Privately Owned			0
- Single Owner		6	6
- Common Ownership (HOA)		4	4
- Multiple Owners		2	2
Proximity to SFR*	20%	20	22
Width of Corridor - Separation		15	15
Elevation - Visibility from Above		-5	-5
Buffers		10	7
- Visual		6	4.25
- Vegetation		1.75	1.75
- Fencing		1.75	1.75
- Berms		2.5	2.5
- Noise		4	2.75
- Vegetation		1.25	1.25
- Fencing		1.25	1.25
- Berms		1.5	1.5
Current Conditions	10%	10	10
Ex. Trail or Sidewalk & Used		10	10
No Trail or Sidewalk, but Used		7	7
No Trail or Sidewalk & Un-Used		5	5
Site Details	5%	5	5
Usable w/out Improvement		5	5
Un-Usable w/ out Improvement		2	2
Public Opinion of APO**	25%	25	0
Support (75%+)		25	25
Oppose (<25%)		0	0
Mix - For vs. Against		10 - 15	10 - 15
Jurisdiction	5%	5	5
In City		5	5
In ETJ		3	3
Total	100%	100	67

* Single Family Residential Property ** Adjacent Property Owners

Brushy Creek Sports Park Trails

Although the existing Brushy Creek Regional Trail passes through the southern portion of this park, trails are recommended in the northern part to connect to the potential Cap Metro Rail Trail, Parmer Lane and Brushy Creek Road.



School Trails

Developer trails are proposed through the Leander ISD school properties and two private property lots. These trails will provide safe, off-street, scenic routes between Park Street and Brushy Creek Road.

CITY OF CEDAR PARK - EVALUATION CRITERIA for HIKE & BIKE CORRIDORS				
Corridor Name: School Trails		Score: C		
Type: Developer Trails		Length: 7,945 ft.		
Evaluation Criterion	Importance	Total Pts Available	Points	
Connectivity	20%	20	12	
Schools		6	6	
Trail-to-Trail		6	6	
Parks & Other Amenities		4		
Major Retail		2		
Major Employers		2		
Availability	15%	15	10	
City Owned		15		
Entity Owned		10	10	
Privately Owned			0	
- Single Owner		6		
- Common Ownership (HOA)		4		
- Multiple Owners		2		
Proximity to SFR*	20%	20	18	
Width of Corridor - Separation		15	15	
Elevation - Visibility from Above		-5		
Buffers		10	3	
- Visual		6	1.75	
- Vegetation		1.75	1.75	
- Fencing		1.75		
- Berms		2.5		
- Noise		4	1.25	
- Vegetation		1.25	1.25	
- Fencing		1.25		
- Berms		1.5		
Current Conditions	10%	10	5	
Ex. Trail or Sidewalk & Used		10		
No Trail or Sidewalk, but Used		7		
No Trail or Sidewalk & Un-Used		5	5	
Site Details	5%	5	2	
Usable w/out Improvement		5		
Un-Usable w/ out Improvement		2	2	
Public Opinion of APO**	25%	25	0	
Support (75%+)		25		
Oppose (<25%)		0		
Mix - For vs. Against		10 - 15		
Jurisdiction	5%	5	5	
In City		5	5	
In ETJ		3		
Total	100%	100	52	

* Single Family Residential Property ** Adjacent Property Owners





CITY of CEDAR PARK - EVALUATION CRITERIA for HIKE & BIKE CORRIDORS
 Corridor Name: Parmer Lane (Park St to Brushy Creek Rd) Score: A
 Type: Parkway Sidewalk Length: 7,500 ft.

Evaluation Criterion	Importance	Total Pts Available	Points
Connectivity	20%	20	10
Schools		6	6
Trail-to-Trail		6	6
Parks & Other Amenities		4	4
Major Retail		2	2
Major Employers		2	2
Availability	15%	15	10
City Owned		15	10
Entity Owned		10	10
Privately Owned			0
- Single Owner		6	6
- Common Ownership (HOA)		4	4
- Multiple Owners		2	2
Proximity to SFR*	20%	20	25
Width of Corridor - Separation		15	15
Elevation - Visibility from Above		-5	0
Buffers		10	10
- Visual		6	6
- Vegetation		1.75	1.75
- Fencing		1.75	1.75
- Berms		2.5	2.5
- Noise		4	4
- Vegetation		1.25	1.25
- Fencing		1.25	1.25
- Berms		1.5	1.5
Current Conditions	10%	10	7
Ex. Trail or Sidewalk & Used		10	10
No Trail or Sidewalk, but Used		7	7
No Trail or Sidewalk & Un-Used		5	5
Site Details	5%	5	2
Usable w/out Improvement		5	5
Un-Usable w/ out Improvement		2	2
Public Opinion of APO**	25%	25	25
Support (75%+)		25	25
Oppose (<25%)		0	0
Mix - For vs. Against		10 - 15	10 - 15
Jurisdiction	5%	5	5
In City		5	5
In ETJ		3	3
Total	100%	100	84

* Single Family Residential Property ** Adjacent Property Owners

Parmer Lane (Park St. to Brushy Creek Rd.)

A parkway sidewalk is proposed for both sides of Parmer Lane from Park St. to Brushy Creek Rd. This will provide a connection from the north part of the City to the existing Brushy Creek Regional Trail.



Gas Line Trail (west of Parmer Lane)

As mentioned previously, the Lone Star Gas Line easement extends through the western half of the City. This section of the gas line trail will provide a connection from Parmer Lane to the neighborhoods to the west. Although the trail passes along the gas line easement, the actual home owners' property goes to the centerline of the easement. This section of the gas line easement trail will then be difficult to construct because an agreement will be needed by all the homeowners.

CITY OF CEDAR PARK - EVALUATION CRITERIA for HIKE & BIKE CORRIDORS			
Corridor Name: Gas Line Trail (west of Parmer Lane)		Score: D	
Type: Trail		Length: 5,435 ft.	
Evaluation Criterion	Importance	Total Pts Available	Points
Connectivity	20%	20	4
Schools		6	
Trail-to-Trail		6	
Parks & Other Amenities		4	4
Major Retail		2	
Major Employers		2	
Availability	15%	15	2
City Owned		15	
Entity Owned		10	
Privately Owned			2
- Single Owner		6	
- Common Ownership (HOA)		4	
- Multiple Owners		2	2
Proximity to SFR*	20%	20	13
Width of Corridor - Separation		15	15
Elevation - Visibility from Above		-5	-5
Buffers		10	3
- Visual		6	1.75
- Vegetation		1.75	1.75
- Fencing		1.75	
- Berms		2.5	
- Noise		4	1.25
- Vegetation		1.25	1.25
- Fencing		1.25	
- Berms		1.5	
Current Conditions	10%	10	5
Ex. Trail or Sidewalk & Used		10	
No Trail or Sidewalk, but Used		7	
No Trail or Sidewalk & Un-Used		5	5
Site Details	5%	5	2
Usable w/out Improvement		5	
Un-Usable w/ out Improvement		2	2
Public Opinion of APO**	25%	25	0
Support (75%+)		25	
Oppose (<25%)		0	
Mix - For vs. Against		10 - 15	
Jurisdiction	5%	5	5
In City		5	5
In ETJ		3	
Total	100%	100	31

* Single Family Residential Property ** Adjacent Property Owners





CITY of CEDAR PARK - EVALUATION CRITERIA for HIKE & BIKE CORRIDORS
 Corridor Name: Silverado Springs Park South
 Type: Trail
 Score: A
 Length: 2,615 ft.

Evaluation Criterion	Importance	Total Pts Available	Points
Connectivity	20%	20	10
Schools		6	6
Trail-to-Trail		6	6
Parks & Other Amenities		4	4
Major Retail		2	2
Major Employers		2	2
Availability	15%	15	15
City Owned		15	15
Entity Owned		10	10
Privately Owned			0
- Single Owner		6	6
- Common Ownership (HOA)		4	4
- Multiple Owners		2	2
Proximity to SFR*	20%	20	21
Width of Corridor - Separation		15	15
Elevation - Visibility from Above		-5	0
Buffers		10	6
- Visual		6	3.5
- Vegetation		1.75	1.75
- Fencing		1.75	1.75
- Berms		2.5	2.5
- Noise		4	2.5
- Vegetation		1.25	1.25
- Fencing		1.25	1.25
- Berms		1.5	1.5
Current Conditions	10%	10	7
Ex. Trail or Sidewalk & Used		10	10
No Trail or Sidewalk, but Used		7	7
No Trail or Sidewalk & Un-Used		5	5
Site Details	5%	5	2
Usable w/out Improvement		5	5
Un-Usable w/ out Improvement		2	2
Public Opinion of APO**	25%	25	25
Support (75%+)		25	25
Oppose (<25%)		0	0
Mix - For vs. Against		10 - 15	10
Jurisdiction	5%	5	5
In City		5	5
In ETJ		3	3
Total	100%	100	85

* Single Family Residential Property ** Adjacent Property Owners

Silverado Springs Park South

Silverado Springs Park South currently has trails through half of the park site. This Master Plan proposes looping the trail through the park and connecting it north along the greenbelt to Turkey Path Bend. This will provide a connection from the Silverado Springs neighborhood to the park.



Eastside Developer Trails

Similar to areas in Sector 3, there are several undeveloped lands in eastern portion of the City. As these areas are developed in the future, developer trails are proposed to connect those residential homes to the overall trail system. One significant trail connection will be providing a safe pedestrian crossing over Brushy Creek Road to connect to the existing Brushy Creek Regional Trail. A pedestrian underpass is proposed as part of the Brushy Creek Road plan.

CITY OF CEDAR PARK - EVALUATION CRITERIA for HIKE & BIKE CORRIDORS			
Corridor Name: Eastside Developer Trails		Score: D	
Type: Developer Trails		Length: 18,060 ft.	
Evaluation Criterion	Importance	Total Pts Available	Points
Connectivity	20%	20	10
Schools		6	6
Trail-to-Trail		6	6
Parks & Other Amenities		4	4
Major Retail		2	2
Major Employers		2	2
Availability	15%	15	0
City Owned		15	0
Entity Owned		10	0
Privately Owned		0	0
- Single Owner		6	0
- Common Ownership (HOA)		4	0
- Multiple Owners		2	2
Proximity to SFR*	20%	20	18
Width of Corridor - Separation		15	15
Elevation - Visibility from Above		-5	0
Buffers		10	3
- Visual		6	1.75
- Vegetation		1.75	1.75
- Fencing		1.75	0
- Berms		2.5	0
- Noise		4	1.25
- Vegetation		1.25	1.25
- Fencing		1.25	0
- Berms		1.5	0
Current Conditions	10%	10	5
Ex. Trail or Sidewalk & Used		10	0
No Trail or Sidewalk, but Used		7	0
No Trail or Sidewalk & Un-Used		5	5
Site Details	5%	5	2
Usable w/out Improvement		5	0
Un-Usable w/ out Improvement		2	2
Public Opinion of APO**	25%	25	0
Support (75%+)		25	0
Oppose (<25%)		0	0
Mix - For vs. Against		10 - 15	0
Jurisdiction	5%	5	5
In City		5	5
In ETJ		3	0
Total	100%	100	40

* Single Family Residential Property ** Adjacent Property Owners





CITY of CEDAR PARK - EVALUATION CRITERIA for HIKE & BIKE CORRIDORS
 Corridor Name: Silverado Springs Neighborhood
 Type: Sidewalk
 Score: C
 Length: 10,230 ft.

Evaluation Criterion	Importance	Total Pts Available	Points
Connectivity	20%	20	6
Schools		6	6
Trail-to-Trail		6	6
Parks & Other Amenities		4	
Major Retail		2	
Major Employers		2	
Availability	15%	15	15
City Owned		15	15
Entity Owned		10	
Privately Owned			0
- Single Owner		6	
- Common Ownership (HOA)		4	
- Multiple Owners		2	
Proximity to SFR*	20%	20	21
Width of Corridor - Separation		15	15
Elevation - Visibility from Above		-5	
Buffers		10	6
- Visual		6	3.5
- Vegetation		1.75	1.75
- Fencing		1.75	1.75
- Berms		2.5	
- Noise		4	2.5
- Vegetation		1.25	1.25
- Fencing		1.25	1.25
- Berms		1.5	
Current Conditions	10%	10	5
Ex. Trail or Sidewalk & Used		10	
No Trail or Sidewalk, but Used		7	
No Trail or Sidewalk & Un-Used		5	5
Site Details	5%	5	2
Usable w/out Improvement		5	
Un-Usable w/ out Improvement		2	2
Public Opinion of APO**	25%	25	0
Support (75%+)		25	
Oppose (<25%)		0	
Mix - For vs. Against		10 - 15	
Jurisdiction	5%	5	5
In City		5	5
In ETJ		3	
Total	100%	100	54

* Single Family Residential Property ** Adjacent Property Owners

Silverado Springs Neighborhood

Sidewalks are proposed along the major collector streets throughout the Silverado Springs neighborhood. The majority of streets in the City have sidewalks, so it is likely the developer will construct these sidewalks as new homes are built.



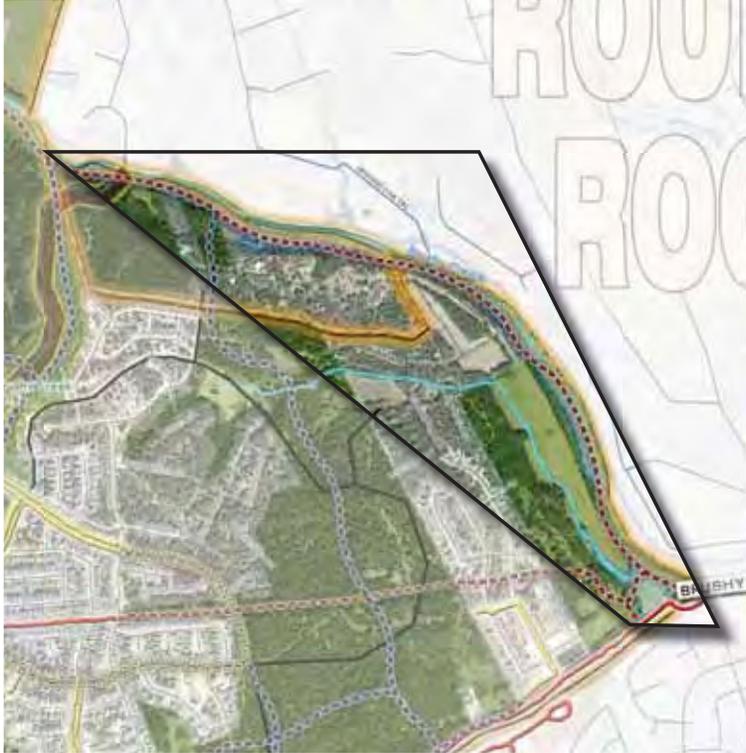
Gas Line Trail (east of Parmer Lane)

This segment of the gas line easement trail will be much easier to construct because the easement is designated as its own parcel; therefore, there is not the difficulty of getting an agreement from various property owners. A portion of this trail is already in place and was built by the neighborhood developer.

CITY OF CEDAR PARK - EVALUATION CRITERIA for HIKE & BIKE CORRIDORS			
Corridor Name: Gas Line Trail (east of Parmer Lane)		Score: B	
Type: Trail		Length: 7,095 ft.	
Evaluation Criterion	Importance	Total Pts Available	Points
Connectivity	20%	20	6
Schools		6	
Trail-to-Trail		6	6
Parks & Other Amenities		4	
Major Retail		2	
Major Employers		2	
Availability	15%	15	10
City Owned		15	
Entity Owned		10	10
Privately Owned			0
- Single Owner		6	
- Common Ownership (HOA)		4	
- Multiple Owners		2	
Proximity to SFR*	20%	20	16
Width of Corridor - Separation		15	15
Elevation - Visibility from Above		-5	-5
Buffers		10	6
- Visual		6	3.5
- Vegetation		1.75	1.75
- Fencing		1.75	1.75
- Berms		2.5	
- Noise		4	2.5
- Vegetation		1.25	1.25
- Fencing		1.25	1.25
- Berms		1.5	
Current Conditions	10%	10	7
Ex. Trail or Sidewalk & Used		10	
No Trail or Sidewalk, but Used		7	7
No Trail or Sidewalk & Un-Used		5	
Site Details	5%	5	5
Usable w/out Improvement		5	5
Un-Usable w/ out Improvement		2	
Public Opinion of APO**	25%	25	25
Support (75%+)		25	25
Oppose (<25%)		0	
Mix - For vs. Against		10 - 15	
Jurisdiction	5%	5	5
In City		5	5
In ETJ		3	
Total	100%	100	74

* Single Family Residential Property ** Adjacent Property Owners



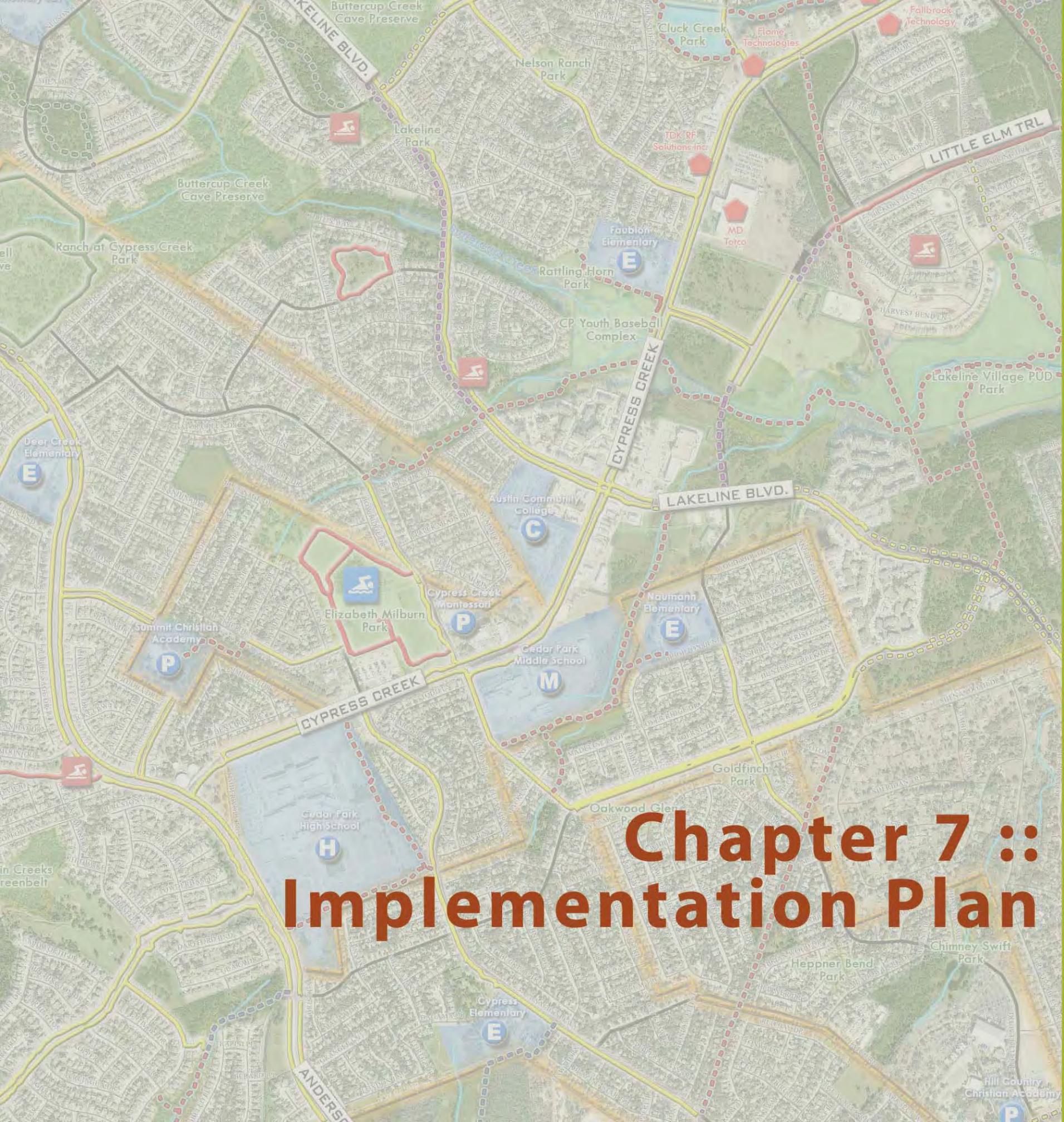


CITY of CEDAR PARK - EVALUATION CRITERIA for HIKE & BIKE CORRIDORS			
Corridor Name: North Fork Brushy Creek		Score: C	
Type: Trail		Length: 9,555 ft.	
Evaluation Criterion	Importance	Total Pts Available	Points
Connectivity	20%	20	10
Schools		6	6
Trail-to-Trail		6	6
Parks & Other Amenities		4	4
Major Retail		2	2
Major Employers		2	2
Availability	15%	15	2
City Owned		15	15
Entity Owned		10	10
Privately Owned			2
- Single Owner		6	6
- Common Ownership (HOA)		4	4
- Multiple Owners		2	2
Proximity to SFR*	20%	20	22
Width of Corridor - Separation		15	15
Elevation - Visibility from Above		-5	0
Buffers		10	7
- Visual		6	4.25
- Vegetation		1.75	1.75
- Fencing		1.75	1.75
- Berms		2.5	2.5
- Noise		4	2.75
- Vegetation		1.25	1.25
- Fencing		1.25	1.25
- Berms		1.5	1.5
Current Conditions	10%	10	5
Ex. Trail or Sidewalk & Used		10	10
No Trail or Sidewalk, but Used		7	7
No Trail or Sidewalk & Un-Used		5	5
Site Details	5%	5	2
Usable w/out Improvement		5	5
Un-Usable w/ out Improvement		2	2
Public Opinion of APO**	25%	25	0
Support (75%+)		25	25
Oppose (<25%)		0	0
Mix - For vs. Against		10 - 15	10 - 15
Jurisdiction	5%	5	4
In City		5	4
In ETJ		3	3
Total	100%	100	45

* Single Family Residential Property ** Adjacent Property Owners

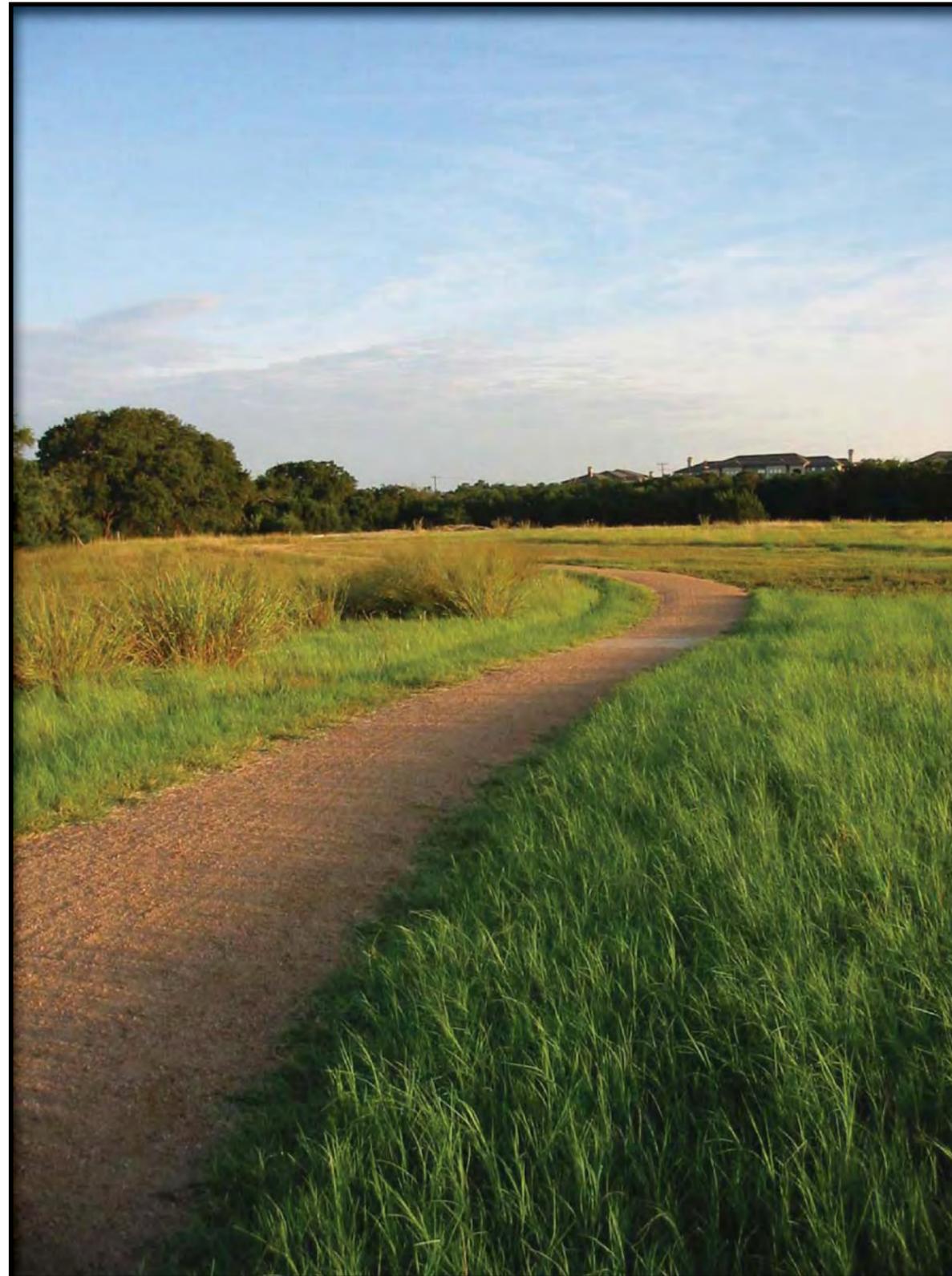
North Fork Brushy Creek

A trail is proposed along the north fork of Brushy Creek. There are several private, large lot property owners along this corridor, which could make construction of a trail difficult. It is important that if these properties were ever sold for future development, that the City maintain ownership of the creek and floodplain. Future parcels should not end at the centerline of the creek. This is already being demonstrated by the Walsh Trails neighborhood. These parcels stop before the floodplain. This practice will make it easier for the City to develop a trail in the future.



Chapter 7 :: Implementation Plan





Key Trail Corridor Recommendations

As shown in the previous chapter, there are many opportunities for trails in Cedar Park. Over the next two to three decades, it is anticipated that many of those opportunities can actually be converted into trails. However, the City's efforts should be focused on those corridors that provide the most significant beneficial impact, and that truly begin to create a major citywide network. In effect, the City's efforts should be focused on creating the "spine" network first.

This chapter presents a citywide network of trails, representing the most important trails to be built using prioritization criteria developed for Cedar Park. Cost projections were prepared for each of the recommended trail corridors, allowing for the preparation of an action plan for trail implementation.

These corridors were selected to meet the goals established by the planning effort, and to reflect citizen comments and desires received during the extensive public input process. Those goals included:

- ◆ Linking all parts of the City
- ◆ Providing a variety of trail types
- ◆ Being compatible with adjacent private properties
- ◆ Creating multiple neighborhood access points
- ◆ Including interpretive facilities
- ◆ Considering trails as both transportation and recreation uses
- ◆ Creating aesthetically pleasing trail corridors that enhance Cedar Park

The high priority proposed trails system network is shown on Page 7-5.

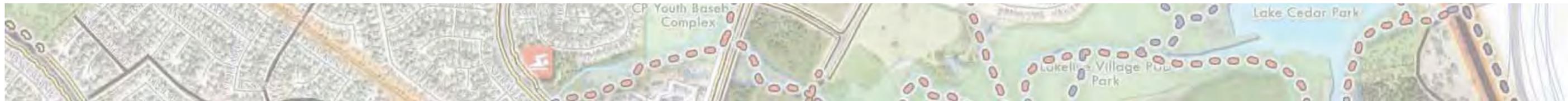


Table 7 - 1
Proposed Parkway Sidewalks that are Part of the City's Transportation Master Plan

Priority	Name	Length (priority portion only)	Type	Recommended Material	Recommended Width
A	New Hope Drive	6,710 linear feet	Parkway Sidewalk	Concrete	10 feet, minimum
B	Lakeline Blvd.	17,535 linear feet	Parkway Sidewalk	Concrete	10 feet, minimum
C	Little Elm Trail	2,885 linear feet	Parkway Sidewalk	Concrete	10 feet, minimum
D	Park Street	11,580 linear feet	Parkway Sidewalk	Concrete	10 feet, minimum
E	Whitestone Blvd.	7,710 linear feet	Parkway Sidewalk	Concrete	10 feet, minimum
F	Brushy Creek Road	2,140 linear feet	Parkway Sidewalk	Concrete	10 feet, minimum

Table 7 - 2
Proposed Priority Sidewalks Built as Part of Neighborhoods (by development)

Priority	Name	Length (priority portion only)	Type	Recommended Material	Recommended Width
G	Discovery Blvd.	7,900 linear feet	Sidewalk	Concrete	8 to 10 feet preferred
H	Silverado Springs Neighborhood	4,230 linear feet	Sidewalk	Concrete	8 to 10 feet preferred
I	Proposed Collector Trail	4,665 linear feet	Sidewalk	Concrete	8 to 10 feet preferred

Table 7 - 3
Proposed Funded Trails

Priority	Name	Length (priority portion only)	Type	Recommended Material	Recommended Width
J	183A Toll Road	8,350 linear feet	Parkway Sidewalk	Concrete	10 feet

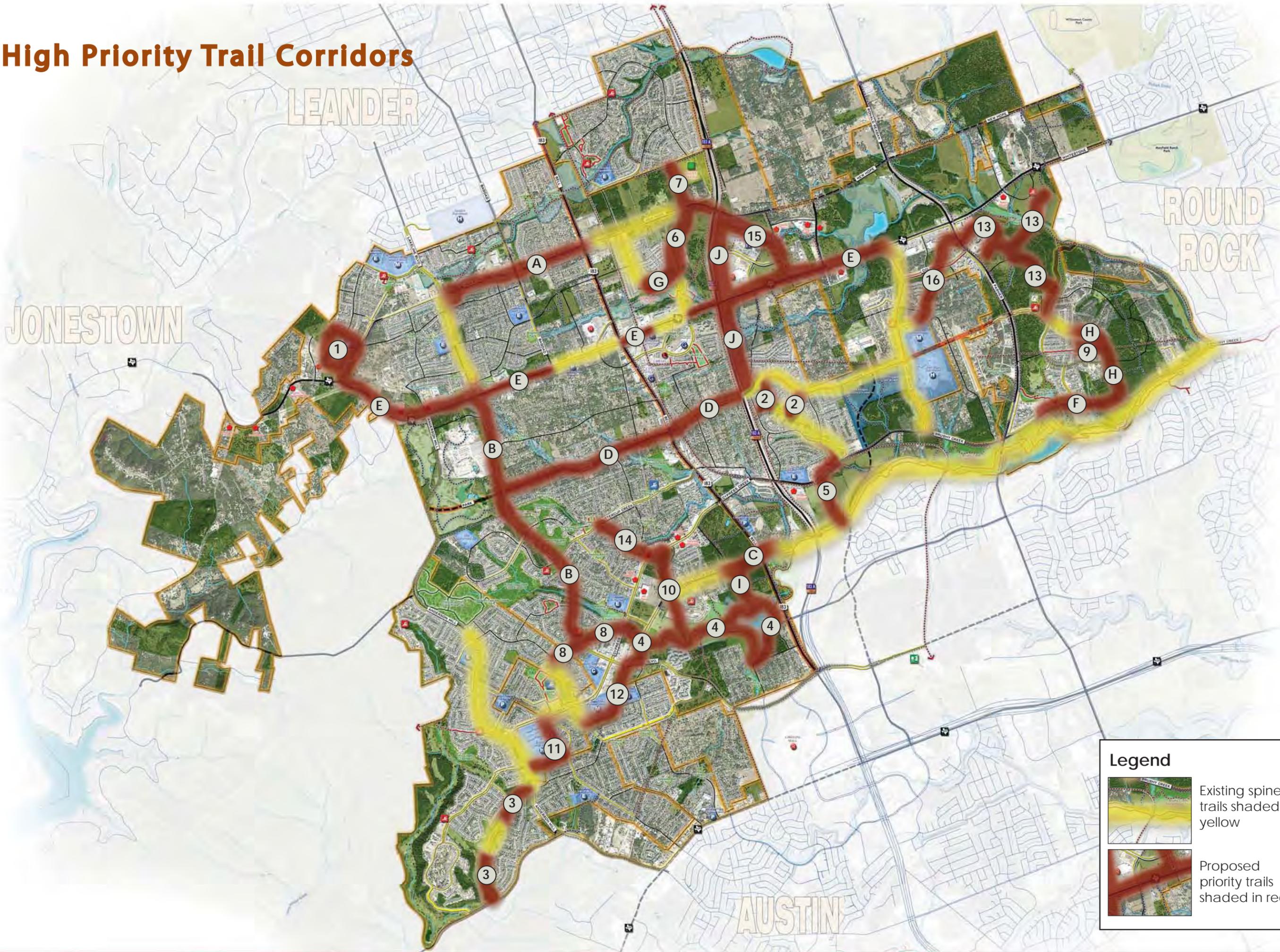


**Table 7 - 4
High Priority Trails**

Priority #	Name	Sector	Length (priority portion only)	Type	Primary Responsibility	Recommended Material	Recom. Width	Potential Cost Range (Low - High)	Timeframe
1	Veterans Memorial Park	1	5,742 linear feet	Trail	City of Cedar Park	Decomposed Granite, Concrete, Nature Trails	10 feet	\$400,000 - \$800,000	2010 - 2015
2	Forest Oaks Park Greenbelt	4	5,617 linear feet	Trail	City of Cedar Park	Concrete	10 feet	\$725,000 - \$1,100,000	2010 - 2015
3	Twin Creeks Historic Park Trail	2	5,902 linear feet	Trail	City of Cedar Park	Decomposed Granite, Concrete, Nature Trails	10 feet	\$400,000 - \$825,000	2010 - 2015
4	Lakeline Village PUD Park	2	15,453 linear feet	Trail	City of Cedar Park	Decomposed Granite	10 feet	\$1,100,000 - \$2,100,000	2010 - 2015
5	Forest Oaks to Brushy Creek Trail	4	5,712 linear feet	Trail	City of Cedar Park	Concrete	10 feet	\$750,000 - \$1,200,000	2010 - 2015
6	Town Center Median Trail	3	2,905 linear feet	Trail	City of Cedar Park	Decomposed Granite	10 feet	\$200,000 - \$400,000	2010 - 2015
7	Event Center Trails	3	1,967 linear feet	Developer Trail	Developer	Concrete	10 feet	\$250,000 - \$350,000	2010 - 2015
8	South Buttercup Creek Trail	2	5,693 linear feet	Trail	City of Cedar Park	Concrete	10 feet	\$750,000 - \$1,200,000	2016 - 2020
9	East Gas Line Trail	4	1,245 linear feet	Trail/Developer	City of Cedar Park	Decomposed Granite	10 feet	\$90,000 - \$175,000	2016 - 2020
10	Lakeline Village Powerline Corridor	2	6,774 linear feet	Trail	City of Cedar Park	Concrete	10 feet	\$880,000 - \$1,400,000	2016 - 2020
11	Cedar Park High School Trail	2	3,832 linear feet	Trail	City of Cedar Park/LISD	Concrete	10 feet	\$500,000 - \$750,000	2016 - 2020
12	School Drainage	2	5,658 linear feet	Trail	City of Cedar Park/LISD	Concrete	10 feet	\$725,000 - \$1,000,000	2016 - 2020
13	Eastern Developer Trails	3	12,350 linear feet	Developer Trail	Developer	Decomposed Granite	10 feet	\$850,000 - \$1,700,000	Beyond 2020
14	Cluck Creek Trail	2	5,670 linear feet	Trail	City of Cedar Park	Concrete	10 feet	\$750,000 - \$1,100,000	Beyond 2020
15	Medical Center Area Trails	3	4,474 linear feet	Trail	City of Cedar Park	Decomposed Granite	10 feet	\$300,000 - \$625,000	Beyond 2020
16	Silverado Springs Drainage Corridor	3	4,875 linear feet	Trail	City of Cedar Park	Concrete	10 feet	\$625,000 - \$925,000	Beyond 2020

In providing opinions of probable construction cost, the Client understands that the Design Professional has no control over costs or the price of labor, equipment or materials, or over the Contractor's method of pricing, and that the opinions of probable construction costs provided herein are to be made on the basis of the Design Professional's qualifications and experience. The Design Professional makes no warrant, expressed or implied, as to the accuracy of such opinions as compared to bid or actual costs.

High Priority Trail Corridors



Legend

	Existing spine trails shaded in yellow
	Proposed priority trails shaded in red



Implementation Process

An action plan designated for the implementation of each specific trail corridor should coordinate all of the following steps:

- ◆ **Preliminary items** - Environmental analysis, property easement or right of way needs analysis, preliminary concept design, possible feasibility study, allocation of general budget - all these should be obtained before proceeding.
- ◆ **Permits** - By City of Cedar Park, possibly Williamson County, and all involved trail corridor owners, e.g. TxDOT, utility companies and pipeline companies. Responsibility for the project construction lies primarily with the City of Cedar Park.
- ◆ **Funding** - Research for necessary grant qualification, Council approval to apply for grants or other funding sources, and ROW issues should be settled at this point.
- ◆ **Design** - Preparation of construction documents, specifications and cost estimates, followed by bid documents and bidding procedures after permits and funding are clarified.
- ◆ **Physical construction** of the project.

Coordination with Ongoing and Future Transportation and Drainage Improvements

Major public works improvements such as new street development or drainage facilities can provide an opportunity for trail development. The resurfacing of roads can be used to consider adding bicycle lanes. New roads can be sized to include bicycle lanes or to have side paths built as the road is built. When large new public facilities are being built, trail opportunities along their edges should be considered. Drainage channels can be planned in such a manner that they include trails along one or both sides, and can be oriented so that adjacent homes are not impacted.

Every effort in the City, whether private or public, whether funded by the City or by another agency such as Williamson County, should be considered early on as a potential bicycle facility or shared use path candidate. Adequate right of way should be acquired early so as to provide corridors for trails. It is extremely difficult to retrofit trails once development around it has occurred.

Private sector developments should be carefully reviewed to determine if key trail corridors shown in this plan can be integrated into the proposed development. In some cases, the City may consider funding portions of the recommended trails over and above the developer portion so as to expedite construction of the overall trail system.

Trail Type-Related Costs

General costs are included for use in planning for trail corridors. However, general costs are always subject to change and will vary as more detailed design occurs.

General estimated construction costs, for use in preliminary project feasibility determinations:

◆ Construction of a new concrete trail, 10 feet wide	\$400,000 to \$600,000 per mile (trail and subsurface only)
◆ On-street trails, striping and signage	\$15,000 per mile
◆ On-street trails, striping only	\$3,500 per mile
◆ Widening of ROW/shoulder (asphalt)	\$220,000 per mile
◆ Soft-surface trail (mulch, sand, gravel)	\$170,000 per mile
◆ At-grade crossing	\$5,000 to \$10,000 each
◆ At-grade crossing, lighted	\$20,000 to \$30,000 each
◆ At-grade crossing, traffic light modification	\$20,000 to \$30,000 each
◆ Below grade crossing	\$100,000 to \$130,000 each
◆ Bridge crossing	\$200,000 to \$250,000 each





Issues Associated with Trail Funding

Funding for trail and greenway corridor development in Cedar Park can come from a variety of sources such as generated locally, from State of Texas, and federal sources. Private development of trails will also aid in the establishment of much of the future trails throughout the City.

Each trail segment will have unique funding opportunities, based on the neighborhoods around the trail and the specific characteristics of the corridor. Key issues associated with funding are as follows:

- ◆ If possible, funding should be continuous and steady. Annual designation of funds for trail development will result in a steady growth in the City's trail system, and allow the citizens of Cedar Park to see a continuous flow of new trail segments every year, rather than in sporadic bursts.
- ◆ Construction of major trail corridors should be the focus of public expenditures. Major "spine" segments that connect neighborhood to neighborhood should be the primary focus of public expenditures for trails. Trails within and primarily serving private developments and individual neighborhoods should be paid for with private sector funds.
- ◆ Funds designated for trail development should not be taken from park development. Both parks and trails are extremely important to the future quality of life in Cedar Park, and funding one should not imply that the other need not be funded.

Sources of Funding

Trails are considered by Cedar Park residents as one of the things they like the most about the City, and as one of their highest priorities. Therefore, funding for trails should be treated as a key item in both annual and longer term budgeting. Regular steady funding is recommended so that the trail system is added to on a continuous basis. A broad range of funding mechanisms, from both the public and private sectors should be considered. These include:

Capital improvement or bond funds - Bond funds are typically the primary source of significant trail development efforts. Larger capacity of these funding sources allows for more development to occur.

Funding as part of other projects - Trails can be efficiently funded as part of other larger city projects, such as new roads. However, separate trail funding should not be added to road projects to help supplement roadway funding that is inadequate to begin with.

Parkland dedication funds - Funds generated by new development can be used to help develop nearby trails. These funds are accrued in lieu of parkland.

Special district funding - Funding from special districts such as the Town Center, other new public improvement areas, or tax increment financing areas can be used to help develop trails.

4B Tax - The Development Corporation Act of 1979, as amended in 1991, allows all cities to adopt the 4B tax, a voter-approved special, dedicated tax that cities can use for economic development purposes. Voters approve the dedication of a portion of the sales tax and the creation of a 4B Corporation to administer the spending of 4B tax funds. The economic development sales tax rate may be 1/8, 1/4, 3/8, or 1/2 of 1 percent if the new total rate of all sales and use taxes would not exceed 2%. 4B Sales Tax may use funds for a wide range of uses intended to give communities an opportunity to undertake a project for quality of life improvements, including economic development that will attract and retain primary employers. Money raised by this tax may be used to acquire or pay for land, buildings, equipment, facilities, expenditures, infrastructure and improvements for purposes related to:

- ◆ Manufacturing and industrial facilities, recycling facilities, distribution centers, small warehouse facilities;
- ◆ Research and development facilities, regional or national corporate headquarters facilities, primary job training facilities for use by institutions of higher education, job training classes; telephone call centers; and career centers that are not located within a junior college taxing district;
- ◆ A general aviation business service airport that is an integral part of an industrial park;
- ◆ Certain infrastructure improvements, which promote or develop new or expanded business enterprises;
- ◆ Port-related facilities to support waterborne commerce;
- ◆ Maintenance and operating costs associated with projects;

◆ Projects that improve a community's quality of life, including parks, professional and amateur sport and athletic facilities, tourism and entertainment facilities, affordable housing, and other improvements or expenditures that promote new or expanded business activity that create or retain primary jobs.

Private residential or commercial development - Many of the trails noted in this master plan are located within residential communities or adjacent to commercial or business areas. As such, trail segments associated with either existing or new development can be partially or entirely built by the private development community. Specific mechanisms to require trail development which can be adopted by the City Council are further discussed in this chapter.

Grants from a variety of sources - Grants that can be used for trail development are available from a variety of sources. The existing remaining bond funds provide an ideal match for grant applications. Given the compelling local issues of traffic congestion and air quality, as well as a large local population that supports alternative transportation methods, local pursuit of grants could be successful and should be aggressively pursued. Major grant types include:

- ◆ **Texas Parks and Wildlife Department grants** - Through its outdoor recreation and community trail development grants, these matching grants can provide from \$50,000 to \$500,000 in grant assistance.
- ◆ **Federal Enhancement funds** - Federal transportation dollars specifically allocated to pay for transportation enhancements have led to the creation of over 100 miles of trails throughout Texas over the past 10 years, and were the primary funding source for trail development in the State of Texas. These funds are administered by the Texas Department of Transportation, and as such must conform to federal guidelines for safety and construction procurement. The locally required match is a minimum of 20%, but communities may overmatch to increase their competitive position. Funds must be reauthorized periodically by the United States Congress, and are currently waiting for re-authorization in the next few years.
- ◆ **Williamson County park and trail development funds** - Williamson County has participated in the development of much of the Brushy Creek regional trail along Brushy Creek. For trail corridors that have regional benefits, Williamson County will continue to be a significant future partner.



- ◆ **Congestion Mitigation and Air Quality (CMAQ) grant funds** - Federal dollars that assist in relieving traffic mitigation may also be used to develop trails corridors that can carry commuters to work or serve as an alternative transportation route to recreation or commercial areas.
- ◆ **Regional Surface Transportation Program (RSTP)** - This is a block grant program that makes money available statewide for roads, bridges, transit capital, bicycle and pedestrian projects. Metropolitan Planning Organizations (MPOs) can transfer money from other federal transportation funding sources to the RSTP program if they want more flexibility in how they allocate their funds. SAFETEA requires states to set aside 10% of the RSTP funds for safety construction activities and another 10% for the Transportation Enhancement Activities (TEA) Program. Applicants eligible for RSTP funds include cities, counties, metropolitan planning organizations (MPOs), transit operators, and the Texas Department of Transportation. Non-profit organizations and special districts also may apply for funds, but they must have a city, county or transit operator sponsor and in some cases administer the project.
- ◆ **Safe Routes to School Program (SR2S)** - The overall purpose of this program is to improve safety in and around school areas. While Safe Routes to School is an overall concept that includes education, enforcements and safety construction improvements, TxDOT's Safe Routes to School Program implemented by HB 2204 will only address safety construction improvements. The rules that established the SR2S Program were adopted by the TxDOT Commission and became effective on July 18, 2002. The following guidelines determine what projects can be submitted: the projects may be located on or off the state highway system, but must be located on public property; must be located within a two mile radius of a school; federal funds requested will be limited to \$500,000; projects can cover multiple school sites if similar work is performed at each site; local project funding match of 20% is required unless the project is located on the state highway system in which case TxDOT will provide the match; a project on the state highway system will not be eligible if the district finds that the project interferes with or disrupts any planned improvements or existing infrastructure. The six categories of work that are eligible for the funding are: sidewalk improvements; pedestrian/bicycle crossing improvements; on-street bicycle facilities; traffic diversion improvements; off-street bicycle and pedestrian facilities; and traffic calming measures for off-system roads.

- ◆ **Hazard Elimination Safety (HES) Program** - This is a federal safety program that provides funds for safety improvements on all public roads and highways. These funds serve to eliminate or reduce the number and/or severity of traffic accidents at locations selected for improvement. The amount of funds allocated to the local HES Program each Federal Fiscal Year may range from \$10 million to \$16 million. Each year, local agencies compete for HES funds by submitting candidate safety projects to TxDOT for review and analysis. TxDOT prioritizes these projects, statewide, and releases an annual HES Program Plan that identifies the projects that are approved for funding.
- ◆ **Foundation and Company Grants** - Some assist in direct funding for trail projects, and some support efforts of non-profit or citizen organizations. Further info can be found at "The Foundation Directory" and at "The Foundation Grants Index" www.fdncenter.org

◆ **"Grants for Greenways"** is a national listing that provides descriptions and links to groups who provide technical and financial support for greenway interests.

Partnering - Partnering with regional volunteer groups can also be helpful when constructing new trail projects. Their efforts can be used as part of the required match for some grants. Partnerships with Utility Companies can often be established for the proposed utility and pipeline easement trails.

◆ Cedar Park volunteer programs, for example through schools or community groups, may substantially reduce the cost of implementing some of the proposed trail segments. Local construction companies might donate or offer discounted services, or local corporations might adopt bikeways, like it is already practiced with highways throughout the area.

**Table 8 - 1
Potential Funding Source Scenario for Trail Development
(Over the next 10 years)**

Funding Type	Potential Funding Range*		Additional Information
	Low	High	
Currently Available Bond Funds	\$600,000	\$600,000	\$600,000 available from 2007 bond election.
Potential Future Bond Funds (Over 10 year timeframe)	\$2,500,000	\$5,000,000	Timing of and inclusion in future bond elections to be determined.
Grants (Potential)			
TPWD	\$250,000	\$1,000,000	Anticipates one trail grant award every five years.
TxDOT Enhancement Funds	\$500,000	\$1,000,000	Requires federal reauthorization of funding.
Other local grant sources	\$500,000	\$750,000	Local public or semi-public entities.
Potential assistance from private non-profit entities	\$100,000	\$400,000	From organizations such as Lions, Kiwanis, Rotary, Junior League, and others.
Potential private non-residential business assistance	\$500,000	\$1,000,000	From a variety of large employers and commercial entities in the City.
Private sector residential trail development	\$1,500,000	\$2,500,000	For major trail segments adjacent to communities.
Total Potential Trail Funding	\$6,450,000	\$12,150,000	

*Amounts shown are used to illustrate a potential trail funding scenario, and do not represent any actual commitment to funds.



Trail Ordinances

Successful implementation of the Trails Master Plan will require the protection of existing trail connections and the reservation of planned trail connections throughout the City. Although many of the trail corridors are intended to utilize public lands consistent with the goals and policies of the Trails Master Plan, acquisition of trail corridors on private lands will be necessary with future development to successfully implement this plan.

The City of Cedar Park's goal is to fund and build the spine of the network as outlined in Chapter 6 while working with private developers and landowners; and encouraging the private sector to develop and build additional parts of the trail system as Cedar Park continues to grow.

Many options are available to the City, public agencies, non-profit groups, and private landowners to ensure the protection/reservation of these critical trail corridors. The objective of the Trails Master Plan is to provide a menu of available options to both public agencies and private landowners, promoting flexibility and creativity in the negotiation process. Careful crafting of transactions between private landowners and public agencies can and should produce mutually beneficial results.

Trail Development Ordinance - Consideration of a trail development ordinance is recommended by the Trails Master Plan. Similar ordinances have been enacted in other cities in Texas, and have proven successful in helping to get trails constructed. The ordinance model used in Allen, Texas requires complete developer construction of key trail segments that fall within their property limits, without city participation. City funding in that city is used for other regional trails or for trailhead development. Often, the required trails replace adjacent sidewalks, and therefore, do not add significantly to the cost of the development. Credits for landscaping, pavement, or other infrastructure elements can be given in return for trail construction. A central point to consider is that most developments will add trails automatically; therefore, such a mandatory trail development ordinance only serves to create a level playing field between the many developments that include trails and those that will build them only if required to do so.

Develop Trail Cost Sharing Ordinance - An alternative type of ordinance is patterned after sidewalk requirements, in which adjacent property owners fund a portion of the trail installation cost, with the City of Cedar Park covering the remainder of the

cost.

New Development Reservations and Dedications - The preservation of trail corridors in conjunction with or independent of the open space areas required to be created with new residential development could be required in the City Code. Right of way reservations for pedestrian paths, bikeways, and multiple use trails could be required of new residential developments consistent with the Engineering Standards and/or this Trails Master Plan. An offer of dedication is required when a reasonable relationship is demonstrated between the need for the dedication and the characteristics and impacts of the proposed development.

The City Code could also provide incentives to new development to encourage implementation of the Trails Master Plan. Reduction in required open space areas and fee waivers are two specific incentives for public trail reservations and dedications beyond that required of any new development. Additional flexibility could be provided for new development, promoting the highest quality development in concert with the public need and benefit derived from creative and innovative development proposals. This flexibility might come by allowing reductions in required off-street parking and flexibility in internal project circulation layout, which is justified with the reservation/dedication of lands in support of the planned recreation trail network.

Existing Development - In cases where trail corridors shown on the Trails Master Plan intersect with existing developed areas, the acquisition of lands will be necessary to create connectivity with adjoining trail corridors. Acquisition can be accomplished through a variety of forms: outright purchase of property, purchase of easements, or donations. These varieties of acquisition may be employed, while always seeking the most cost effective method to secure appropriate public interest when necessary and warranted. Public/private negotiations for outright purchase of private property will be necessary in some instances; however, the purchase of easement or partial/restricted property right at less cost to the public will be encouraged.

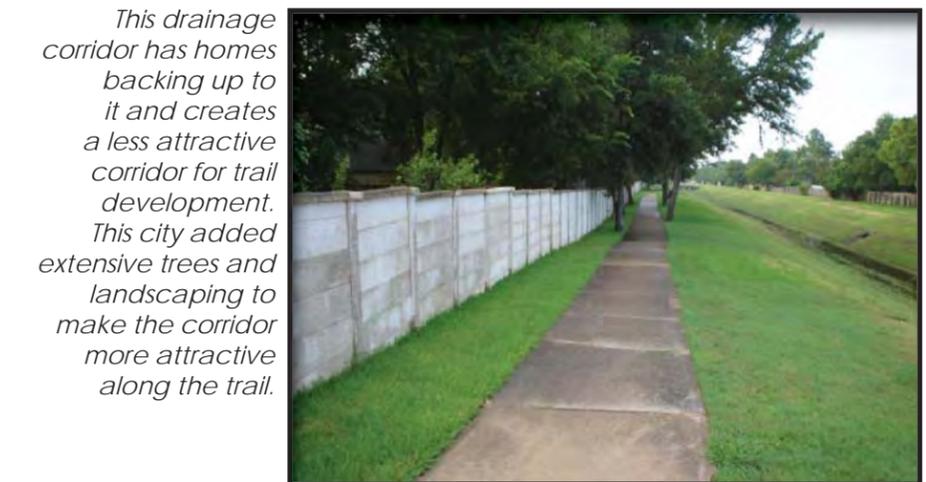
Greenway and Trail Setback Recommendations - The purpose of this recommendation is to address the protection and preservation of greenways, trails, and easements for future trail corridors. This will ease the implementation of the Trails Master Plan by protecting, conserving, and maintaining the abundant qualities of the lands along creeks, rivers and waterways within Cedar Park while increasing transportation and recreation opportunities.

Preservation and Access to Creek Corridors

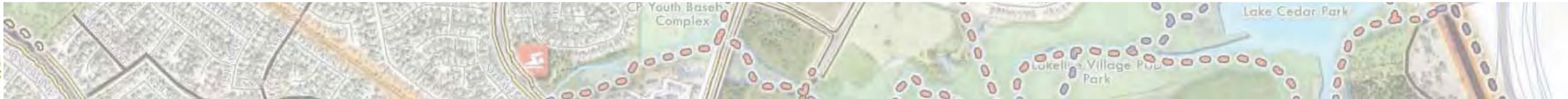
Creek and drainage corridors will be one of the major trail connections within the City, and as such should be developed with access along at least one side of the creek for small drainage tributaries and along both sides of the creek for major creeks such as Brushy Creek. Because they are flood prone areas, these corridors are largely undevelopable, and can preserve much of the remaining natural space in Cedar Park. Steps should be taken to require that natural creek corridors be preserved and trail access be allowed. In most cases, streets paralleling the drainage or creek corridor are preferred, rather than lots that back up to the creek and that effectively seal off the creek from public view or access.



This drainage corridor has a road adjacent to it and is the preferred method of trail development.



This drainage corridor has homes backing up to it and creates a less attractive corridor for trail development. This city added extensive trees and landscaping to make the corridor more attractive along the trail.



Trail Maintenance

Effective trail maintenance is critical to the overall success and safety of trails in Cedar Park. Maintenance activities typically include pavement stabilization, facility upkeep, sign replacement, mowing, litter removal, and painting. A successful maintenance program requires continuity and often involves a high level of citizen participation. Routine maintenance on a year-round basis will not only improve trail safety, but will also prolong the life of the trail. The benefits of good maintenance program are far-reaching, including:

- ◆ A high standard of maintenance is an effective advertisement to promote the trail as a regional and state recreational resource.
- ◆ Good maintenance can be an effective deterrent to vandalism, litter, and encroachments.
- ◆ Good maintenance is necessary to preserve positive public relations between the adjacent land owners and managing agency.
- ◆ Good maintenance can make enforcement of regulations on the trail more efficient. Local clubs and interest groups will take pride in “their” trail and will be more apt to assist in protection of the trail.
- ◆ A proactive maintenance policy will help improve safety along the trail.

Ongoing trail maintenance likely includes some, if not all, of the following activities:

Vegetation - In general, plantings should be placed far enough apart to maintain good visibility and avoid creating the feeling of an enclosed space. This will also give trail users good, clear views of their surroundings, which enhances the aesthetic experience of the trail. Under-story vegetation within most trail right of way should not be allowed to grow higher than 36 inches, except in cases where the under-story vegetation is natural, desirable, and part of the habitat required for wildlife. Trees species selection and placement should be made that minimizes vegetative litter on the trail and root uplifting of pavement. Vertical clearance along the trail should be periodically checked, and any branches hanging over the trail should be pruned to a minimum vertical clearance of 10 feet.

Some basic measures should be taken to protect the trail investment. This includes at a minimum bi-annual mowing along both sides of the trail to prevent invasion of plants into the pavement area. The recommended times of year for minimum mowing are fall and spring. Higher levels of maintenance may be necessary.

Wherever possible, vegetation control should be accomplished by mechanical means, organic means, or hand labor. Some species may require spot application of state-approved herbicide.

Surfacing - Where concrete is the recommended surface material, cracks, ruts, and water damage will need to be repaired periodically.

Where drainage problems exist along the trail, ditches and drainage structures will need to be kept clear of debris to prevent washouts along the trail and maintain positive drainage flow. Checks for erosion along the trail should be made during the wet season, and immediately after any storm that brings flooding to the local area. The use of trails with natural soft surfaces should be minimized and/or prohibited during wet conditions.

The trail surface should be kept free of debris, especially broken glass and other sharp objects, loose gravel, leaves, and stray branches. Trail surfaces should be swept periodically. Soft shoulders should be well maintained to maximize their usability.

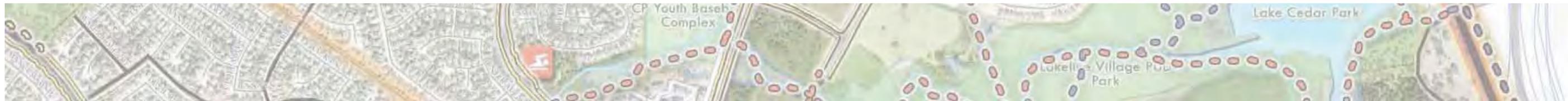
Litter and Illegal Dumping - Staff or volunteers should remove litter along the trail. Litter receptacles should be placed at access points such as trailheads.

Illegal dumping should be controlled by vehicle barriers, regulatory signage, and fines as much as possible. When it does occur, it should be removed as soon as possible in order to prevent further dumping. Neighborhood volunteers, friends groups, alternative community service crews, and inmate labor should be considered in addition to maintenance staff.

Signage - Signage should be replaced along the trail on an as-needed basis.

The following table summarizes the recommended maintenance schedule for the proposed trails in Cedar Park. These guidelines address maintenance for the off-street trails. On-street facilities such as sidewalks and bicycle lanes should be maintained per the standards of the City of Cedar Park.

Item	Frequency
Inspections	Seasonal - at both beginning and end of summer
Signage replacement	1 - 3 years
Pavement markings replacement	1 - 3 years
Major damage response (fallen trees, washouts, flooding)	Schedule based on priorities
Pavement sealing, potholes	5 - 15 years
Introduced tree and shrub plantings, trimming	Every 1 - 3 years
Culvert inspection	Before winter and after major storms
Cleaning ditches	As needed
Trash disposal/litter pick up	Weekly during high use, twice monthly during low use
Mowing (corridor parallel to trail only)	14 to 21 times per year
Lighting luminaire repair	As needed
Pavement sweeping/blowing	As needed, before high use season; weekly in fall
Maintaining culvert inlets	Inspect before the onset of the wet season, then again in early fall
Shoulder plant trimming (weeds, trees, brambles)	Twice a year, middle of growing season and early fall
Waterbar maintenance (earthen trails)	Annually
Site furnishings, replace damaged components	As needed
Graffiti removal	Weekly, as needed
Fencing repair	Inspect monthly for holes and damage, repair immediately
Shrub/tree irrigation for introduced planting areas	Weekly during summer months until plants are established



Implementation Timeframe 2010 - 2020

The overall recommendations of this Trails Master Plan are estimated to take up to 20 years to complete. The following sequence or hierarchy of actions is recommended to implement the Trails Master Plan.

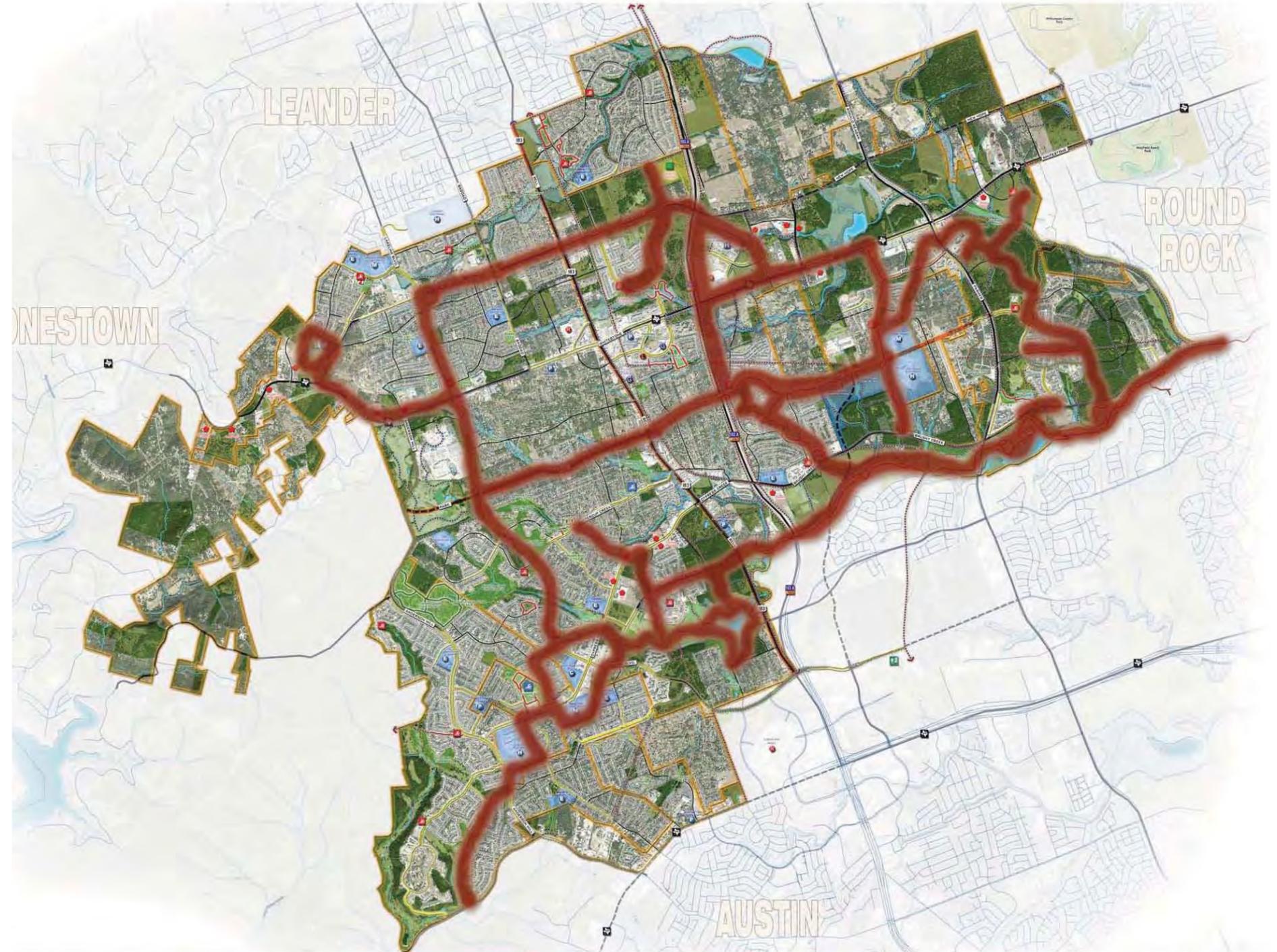
Consider acquisition of trail corridors as the highest priority - Connectivity across the City remains the highest priority of the trails plan, and to accomplish that access trail corridors must be acquired. Creek corridors can be acquired through outright purchase or through access easements. Once a tract of land is developed, it is extraordinarily difficult to acquire land or easements for trail corridors.

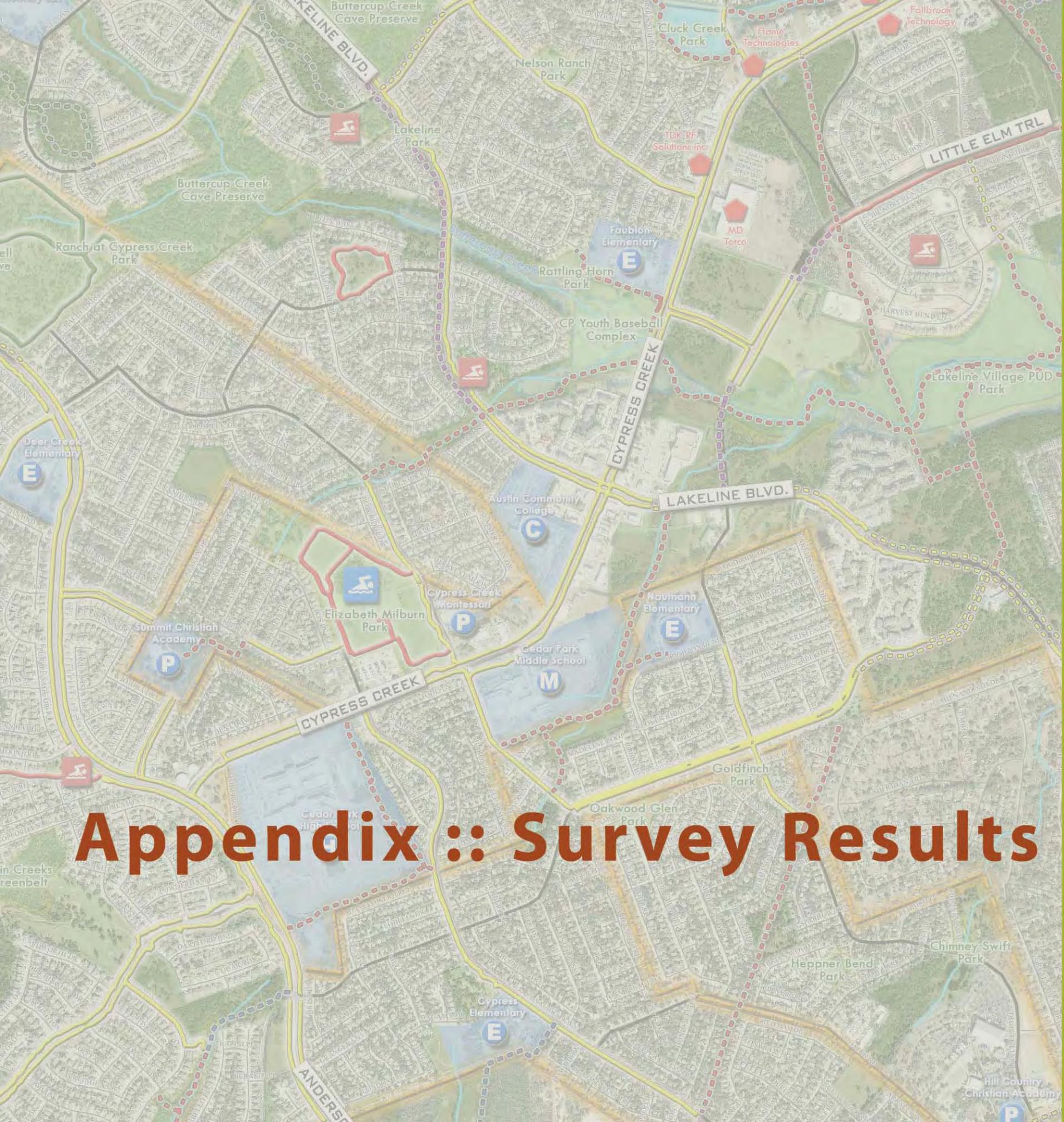
Consider embarking on an extensive trail development schedule over the next 10 years - Cedar Park continues to grow at an unprecedented rate, and demand for quality of life features such as trails will only grow. It is while the City is growing that it becomes the easiest time in which to build trails.

Average the construction of one to two miles of trails per year for the next ten years - Maintain a steady funding channel so that trail development can remain a high priority over the next decade.

Develop strategies to work with private sector development - Voluntary and mandatory processes to work with private development should be put in place immediately, so as to not miss any opportunity to implement segments of trails.

Review and update the citywide Trails Master Plan annually - This Trails Master Plan is a living document, and should be reviewed and updated periodically. This review should occur at the same time that the overall Parks and Recreation Master Plan is being reviewed, so that continuity between the two plans is maintained.





Appendix :: Survey Results





CEDAR PARK 2009 TRAILS MASTER PLAN ATTITUDINAL SURVEY CUMULATIVE RESULTS

RAYMOND TURCO & ASSOCIATES

SEPTEMBER 2009

Cedar Park is currently involved in creating a trails master plan. When completed, this plan will help the city set priorities for trail facilities for the next 5-10 years. Plans of this nature are critical as City leaders strive to improve the quality of life in Cedar Park.

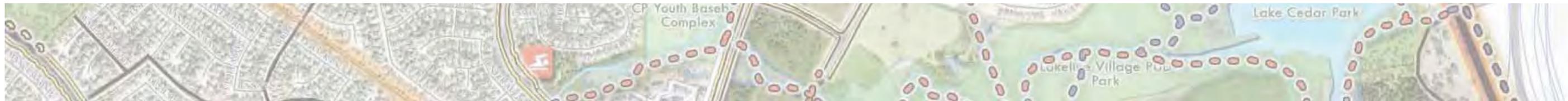
This online survey has been created to allow you and other residents of Cedar Park to comment about trails in the City. The questions are designed to gather your opinions about trails, are completely confidential, and only cumulative results will be used in the master planning effort. Please choose only one answer for each question unless asked to check all that apply. Once all questions on the page have been answered, please click on "Continue" at the bottom of the screen. Choosing to "reset these answers" will clear your answers on that screen only. At any time you may click on the back arrow to go to the previous screen. Thank you for taking the time to answer this survey. The information it provides is invaluable to the City of Cedar Park.

1. Do you live in Cedar Park? (If no, discontinue survey)	
Yes	87%
No	13%
2. What area of the city do you live in? (Please refer to the map below)	
Area 1	13%
Area 2	57%
Area 3	18%
Area 4	12%
3. What is the name of your neighborhood or subdivision?	
Anderson Mill West	1%
Bella Vista	0%
Brushy/Brush Creek	0%
Butter Cup/Butter Cup Creek	11%
Carriage Hills	0%
Cedar Park Town Center	5%
Coventry Crossing	1%
Crossing @ Carriage Hills	1%
Cypress Bend	0%
Cypress Canyon Preserve	1%
Cypress Creek	1%
Cypress Mill	4%
Dear Creek Ranch/Dear Creek	15%
Emerald Oaks	1%
Forest Oaks	8%
Gann Ranch/Preserve at Gann Ranch	1%
Heritage Park	1%
Hunters Glenn	0%
Lakeline Oaks/Lakeline	1%
Miscellaneous	5%
Oakmont Forest/Oakmont	2%

(Question #3 continued)

Oakwood Glen	1%
Park Place I, II, & III	3%
Quarry Oaks	1%
Quest Village/Quest	3%
Ranch at Brushy Creek	3%
Ranch at Cypress Creek	2%
Red Oak/Red Oaks	1%
Rivera Springs	2%
Silver Oak/Silver Oaks	2%
Silverado Ranch	3%
Silverado West	1%
Twin Creeks	2%
Villages at Carriage Hills	1%
Volente Hills	0%
West Park Oaks/West Park Estates	1%
Westside at Buttercup Creek	5%
Westside Preserve	5%
Whitestone Oaks/Whitestone	1%
Willow Oak/Willow Oaks	1%
The Place	0%

4. How long have you lived in Cedar Park?	
Less than 4 years	41%
4 to 10 years	33%
Over 10 years	26%
5. Do you live in a household with children under the age of 18?	
Yes	67%
No	33%
6. In the past 12 months, have you or anyone in your household utilized a trail/bicycle facility in Cedar Park, Williamson County, or elsewhere in Central Texas? (Circle all that apply)	
Cedar Park	77%
Williamson County	53%
Austin	58%
Round Rock	23%
Georgetown	15%
Other	7%
Haven't utilized a trail/ bicycle facility	8%
7. What would you like trails in Cedar Park to connect to? (Check all that apply)	
Surrounding neighborhoods	77%
Schools	45%
Retail shopping	36%
Restaurants	36%
Civic facilities/government buildings	18%
Parks	91%
Places of employment	19%
Library	34%
Recreation center	59%
Other	11%



8. How often do you or your family use trails?

More than once a week	43%
Two times or more a month	26%
Once a month	11%
Several times a year	15%
Once a year	2%
Never	3%

9. When currently using trails, in which of the following activities are you involved? (Check all that apply)

Walking for leisure	66%
Walking/running for exercise	74%
Bike riding	65%
Photography	14%
Bird watching	12%
Wildlife viewing	25%
Mountain bike riding	37%
Other	10%

10. How strongly do you agree or disagree with the following statements:

10a. I would use my bike to get to work if trails were more accessible to my employment area.

Strongly agree	33%
Agree	27%
Disagree	27%
Strongly disagree	13%

10b. I would use my bike (or allow my kids to use their bikes) to get to school if trails were accessible in my neighborhood.

Strongly agree	47%
Agree	40%
Disagree	9%
Strongly disagree	4%

10c. I would support widening some roadways where feasible to allow for bicycle lanes.

Strongly agree	56%
Agree	31%
Disagree	8%
Strongly disagree	5%

10d. I would like to see trails developed as an alternative way to commute or get around Cedar Park.

Strongly agree	58%
Agree	31%
Disagree	8%
Strongly disagree	2%

11. Please tell us how important each of these issues are to you.

	VI	I	U	VU
A) Trails connect to key neighborhood destinations	53%	36%	9%	1%
B) Trails are well-maintained	61%	35%	2%	2%
C) Trails are in places where they will get used	72%	24%	2%	1%
D) There are nice amenities along trails	26%	39%	31%	40%
E) I feel safe along Cedar Park Trails	78%	19%	2%	1%

12. How strongly do you agree or disagree with the following statement: "I would feel comfortable if a hike and bike trail was located adjacent to my home."

Strongly agree	40%
Agree	42%
Disagree	13%
Strongly disagree	5%

13. If a hike and bike trail was built adjacent to your home, what would be your primary concern with it? If you have no issue with a trail being adjacent to your home, write "no issue" in the box below.

No issue	45%
Safety issues/crime	25%
Litter/maintenance issues	13%
Loss of privacy	6%
Miscellaneous	3%
Barking dogs/noise	3%
Strangers/undesirables	2%
Parking issues/traffic	2%
Light pollution	0%

14. Construction of a trail system may also require additional funding. How strongly would you support or oppose a future bond election to finance the construction of trails in Cedar Park?

Strongly support	40%
Support	51%
Oppose	5%
Strongly oppose	4%

15. What is your favorite trail in this area? Please choose your favorite trail from the selections below:

Brushy Creek Trail - crushed granite	27%
Brushy Creek Trail - concrete	15%
Lake Creek Trail in Round Rock	6%
San Gabriel River Trail in Georgetown	5%
Town Lake Trail in Austin	25%
Barton Creek Greenbelt Trail in Austin	22%

16. What are your other favorite trails in this area? Please list them in the space below:

Brushy Creek	28%
Barton Creek Greenbelt	3%
Cave Preserve Trails	1%
Cedar Elm Trail	1%
Lakeway Trail	2%
Milburn Park	4%
Miscellaneous	16%
San Gabriel Trail	2%
St. Edwards Trail	1%
Steiner Ranch Trails	1%
Town Lake	15%
Turkey Creek Nature Trail	2%
(Emma Long Park)	
Walnut Creek	18%
Westside	2%
Willaimson County Park Trail	4%



17. Which types of trails do you prefer in your neighborhood? (Check all that apply)

Multi Purpose/Concrete	53%
Soft Surface Nature Trail	32%
Parkway Sidewalk Trail	25%
Crushed Granite Trail	71%
On-street Bicycle Lane	32%
Neighborhood Sidewalk	20%

18. Where would you prefer trails go in your neighborhood? (Check all that apply)

Along Utility Corridors	50%
Along Major Roads	36%
Along Man-Made Drainage Areas	49%
Along Neighborhood Streets	32%
On Street for Bicycles	37%
Alongside Natural Areas/Creeks	80%

19. What type of activities would you like to use trails in Cedar Park for? (Check all that apply)

Walking for leisure	77%
Walking/running for exercise	83%
Bike riding	76%
Photography	20%
Bird watching	17%
Wildlife viewing	34%
Mountain bike riding	44%
Other	12%

20. Which types of trail amenities would you like to see along trails in Cedar Park? (Check all that apply)

Benches	59%
Bike Racks	21%
Drinking Fountains	64%
Shade	87%
Signage	69%
Kiosks	25%
Lighting	61%
Overlooks or Nodes	28%
Emergency Call Box	66%
Public Art	20%
(along trail corridors)	
Pet Waste Pick Up Stand	61%
Other	11%

21. Which types of bicycle facilities would you like to see in Cedar Park? (Check all that apply)

Off Street Bikeway	71%
Paved Multi Use Path	51%
Lane with Shared Use Marking	20%
(Sharrow, on appropriate streets)	
On Street Bike Lane	44%
(on appropriate streets)	
Soft Surface Multi Use Trail	76%
Wide Curb Bike Route	18%
(with no striped bike lane)	
(on appropriate streets)	

22. Which other types of trails would you like to see in Cedar Park? (Check all that apply)

Esplanade	53%
Paddling trail (for canoeing/	55%
kayaking on Brushy Creek Lake)	
Rails to Trails	60%
(Along the Cap Metro Line)	
Equestrian Trail	17%
Boardwalk Trail or Piers	45%
Mountain Biking Trail	58%

Thank you for completing the survey. Please click the link below to return to the department's home page.